

PLANNING & ZONING COMMISSION



Wendy McClure
Commission Chair

~Agenda~

Mike Ray
Staff Liaison

p&z@ci.moscow.id.us

208.883.7008

<http://id-moscow.civicplus.com/agendacenter>

**Wednesday
May 13, 2020**

7:00 PM

**Council Chambers
206 E 3rd Street**

The Moscow Planning and Zoning Commission and Staff welcome you to tonight's meeting. Due to the Governor's Proclamation relating to the conduct of public meetings during the COVID-19 crisis, the meeting will be broadcast and the public is not allowed to attend in person. Citizens wishing to comment on business on the agenda are encouraged to communicate with the Commission by email (p&z@ci.moscow.id.us) in order to respect social distancing protocol. Please note that Planning and Zoning Commission meetings are televised, videotaped and/or recorded. Thank you for your interest in City government.

Call to order

1. Approval of minutes from March 11, 2020 [ACTION ITEM]

ACTION: Approve minutes as presented; approve minutes with amendments; or provide staff further direction.

2. Proposed RV Park at 913 W Palouse River Drive – LUP2020-0001 [ACTION ITEM]:

A proposal for a Recreational Vehicle (RV) Park located within the existing Palouse Hills Mobile Home Park at 913 W Palouse River Drive.

ACTION: Review and approve the preliminary plans for the proposed RV Park; or approve the preliminary plans for the proposed RV Park with conditions; or deny the preliminary plans for the RV Park; or provide Staff further direction as deemed necessary.

3. Proposed RV Park at 713 Brent Drive – LUP2020-0007 [ACTION ITEM]:

A proposal for a Recreational Vehicle (RV) Park located within the existing Bluebird Mobile Home Park at 713 Brent Drive.

ACTION: Review and approve the preliminary plans for the proposed RV Park; or approve the preliminary plans for the proposed RV Park with conditions; or deny the preliminary plans for the RV Park; or provide Staff further direction as deemed necessary.

4. Preliminary Development Review at 201 W Sixth Street – Flatiron Development [ACTION ITEM]

On September 8, 2018, the Urban Renewal Agency published a request for proposals for the disposition and development of the portions of their Sixth and Jackson property that would remain after the development of Hello Walk. At the Agency's February 21, 2019 meeting, the Agency Board selected the proposal from Roderick Olps and directed staff to prepare the Exclusive Negotiation Agreement (ENA) between Mr. Olps and the Agency. On March 7, 2019 the Board approved an Exclusive Negotiation Agreement (ENA) with Mr. Olps for the disposition and development of the Agency's Sixth and Jackson property. Since the subject property is located within the Legacy Crossing Overlay District, the proposed development is subject to the Legacy Crossing Design Guidelines and requires

NOTICE: Moscow City Council Commission meetings can be televised, videotaped and/or recorded. Individuals attending the meeting who require special assistance to accommodate physical, hearing, or other impairments, please contact the City Clerk, at (208) 883-7015 or TDD 883-7019, as soon as possible so that arrangements may be made.

review and approval from the Planning and Zoning Commission. Mr. Olps has submitted development plans to be reviewed by the Commission for conformance with the design guidelines.

ACTION: Conduct the preliminary development review of the proposed Flatiron Development and approve the proposal; approve the proposal with conditions; deny the proposal; or provide Staff further direction as deemed necessary.

5. Announcements

ACTION: Read any correspondence received and provide staff further direction if necessary.

Adjourn

PLANNING & ZONING COMMISSION



Wendy McClure
Commission Chair

~Meeting Minutes~
March 11, 2020

Mike Ray
Staff Liaison

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Hamilton called the meeting to order at 7:00 p.m.

MEMBERS PRESENT: Joel Hamilton, Second Vice Chair; Rich Beebe, Nels Reese, Victoria Seever, Dennis Wilson

MEMBERS ABSENT: Scott Gropp, Wendy McClure, Mike Nelsen, Robb Parish

ALSO IN ATTENDANCE: Mike Ray, Sandra Kelly, Anne Peterson

1. Approval of minutes from February 12, 2020 [ACTION ITEM]

Wilson moved approval of the minutes as presented, seconded by Seever. Motion carried by acclamation.

2. Public Comment

None.

3. Final Subdivision Plat of a parcel located at 1924 Nursery Street – Paradise View Addition – LUP2020-0002 [ACTION ITEM]:

On August 19, 2019 Moscow City Council approved the Paradise View Addition preliminary plat with no conditions. On February 7, 2020 the applicant submitted the final subdivision plat to be reviewed by the Planning and Zoning Commission and City Council.

Ray reviewed the preliminary subdivision action and showed aerial photos of the area. The only changes for final plat are some minor lot line revisions on the southwest townhouse parcels. Staff recommended approval. Commissioners had no questions. Seever moved to recommend approval to Council. Wilson seconded the motion which carried by acclamation.

4. Final Planned Unit Development and Final Subdivision Plat of a parcel located at 1330 Indian Hills Drive – Indian Hills East Addition – LUP2020-0004 [ACTION ITEM]:

On June 17, 2019 Moscow City Council approved the Indian Hills East Addition preliminary plat with two conditions and approved the Indian Hills East Addition Planned Unit Development (PUD) with no conditions. On February 24, 2020 the applicant submitted the final subdivision plat and final PUD to be reviewed by the Planning and Zoning Commission and City Council.

Ray reviewed the preliminary subdivision action including the two conditions of approval that the private street and detention pond be required to meet current City standards. Applicant has made those changes in the final plat so staff recommended approval with no conditions. Commissioners discussed the advantages of narrower street standards. Seever moved to recommend approval to Council, seconded by Wilson. Motion carried by acclamation.

5. Final Draft Subdivision Code Review [ACTION ITEM]

The Commission has been working on updating the City's Subdivision Code for the last year and a half, including the lot division and lot line adjustment process. New subdivision concepts of conservation and traditional subdivisions have been incorporated into the proposed draft as well. The Commission formed a subdivision review committee who met to review the draft subdivision code on October 18,

2019 and November 15, 2019 and provided recommendations for consideration by the Commission. The Commission reviewed the recommendations at their December 11, 2019 meeting and directed staff to amend the ordinance based upon some of the recommendations.

Ray highlighted the modifications included in this draft based on the Commission's last discussion. Following brief discussion, the Commission directed staff to schedule the code amendment for public hearing.

6. FY2021 Commission Budget [ACTION ITEM]:

The FY2021 budget process is underway and the Commission will need to submit their request.

Following brief discussion of the typical expenditures (commissioner name plaques and occasional training), Seever moved approved of the FY21 budget request of \$1500 as proposed by staff. Reese seconded the motion which carried unanimously.

7. Transportation Commission Meeting Report

Hamilton reported the agenda items for tomorrow's meeting include discussion of the Commission's role, responsibilities and membership, stop sign warrant analyses on Third Street, street width standards, and report on a proposed third route for Smart Transit.

8. Announcements

The meeting adjourned at 7:43 p.m.

Wendy McClure, Chair

COMMUNITY PLANNING AND DESIGN DEPARTMENT STAFF REPORT

HEARING DATE: May 13, 2020

GENERAL INFORMATION

Hearing Body: Planning and Zoning Commission

Subject: Application for a Recreational Vehicle (RV) Park at 913 W Palouse River Drive - LUP2020-0001

Attachments:

1. Proposal Letter
2. Site Map
3. Floodplain Map
4. Topographical Map
5. Site Pictures
6. Site Plan

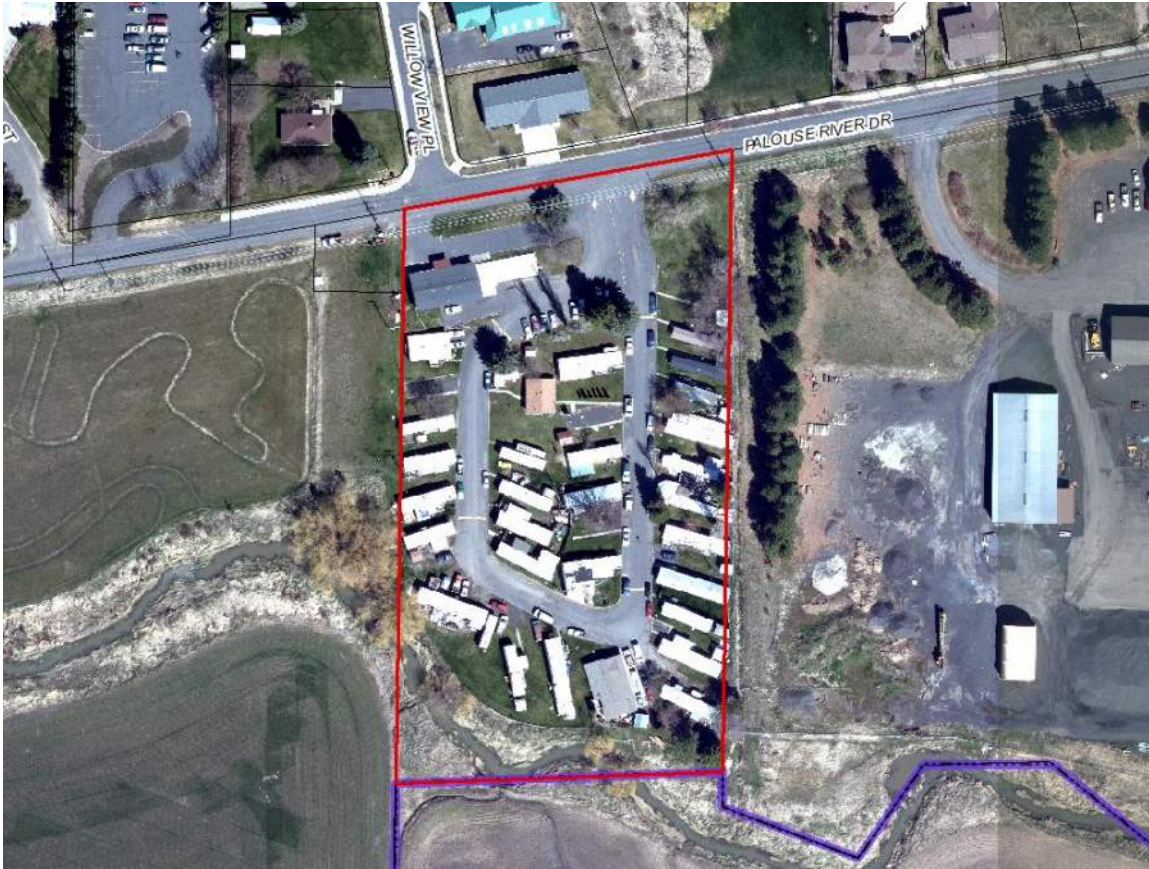
Prepared by: Mike Ray, AICP

STAFF REVIEW

Proposal: The applicant, Dan Gruss, is requesting the allowance of Recreational Vehicle (RV) spaces within the Palouse Hills Mobile Home Community located at 913 W Palouse River Drive. There are currently 33 sites within the Palouse Hills Mobile Home Park and there are a number of sites that are challenging to accommodate mobile homes, but could be suitable for RV spaces. The applicant became more involved in management of the Park in 2015 and has been in the process of improving it. The applicant has indicated that there are about 10 spaces in the park that would be targeted for tiny homes and RVs.

Site and Area Land Use: The subject property was first established as a mobile home park around the 1970's. The subject property is currently 3.94 acres and contains a looped access drive. There are currently 33 spaces within the park and vary between 30' wide by 50' - 70' deep.

Access to the subject property is provided via Palouse River Drive which is designated as a minor arterial. To the east of the subject property is Idaho Transportation Department's maintenance yard, to the north is a residential subdivision and Anderson/Frontier Park, and to the west is the City's Palouse River Drive Ball Fields property. To the south is the South Fork of the Palouse River and the City Limits boundary.



Vicinity Map

Zoning: The subject property and adjacent properties to the east are currently zoned Motor Business (MB). Properties to the north are zoned Low Density, Single Family Residential (R-1) and Medium Density Residential (R-3) and properties to the south within the Area of City Impact are zoned Agriculture/Forestry (AF).

According to the City of Moscow Zoning Code, the purpose of the MB Zoning District is to,

“The MB Zoning District is intended to provide for the location and grouping of compatible uses having similar operation as they involve enterprises which depend primarily on the transient motor vehicle-based trade. By concentrating such uses the City intends to increase public convenience when utilizing such services and, within the regulation, to allow such activities to render the maximum service.”

Recreational Vehicle (RV) parks are a permitted use within the MB Zoning District, but require review and approval by the Planning and Zoning Commission.



Existing Zoning

Comprehensive Plan: The subject property is designated as High-Density Residential upon the 2019 City of Moscow Comprehensive Plan Land Use Map. Surrounding properties to the east are also designated as High-Density Residential, while the City park properties to the west and north are designated as Parks and Open Space. The residential subdivisions to the north are designated as Auto-Urban Residential.

According to the 2019 Comprehensive Plan, the High-Density Residential designated areas are,

“intended to allow the highest density residential development at common densities between 15 and 26 units per acre. Areas appropriate for this designation include those located adjacent to areas of higher activity where there is a range of uses. High-Density Residential areas should be located adjacent or in close proximity to designated arterial or collector streets. New areas of high-density residential development should be located near mixed-use activity areas and corridors that accommodate multi-modal transportation. Lands designated as Urban Residential are generally appropriately zoned Medium Density Residential (R3), Multiple Family Residential (R-4), or Residential Office (RO).”

Access, Streets, Traffic: The subject property has existing access via Palouse River Drive which is developed as a 24 foot wide paved street with gravel shoulders on both sides. Palouse River Drive is designated within the Thoroughfare Plan as a minor arterial.

RV Park Standards:

Size of RV Spaces: The minimum space size within an RV Park shall be 40' x 20'; however, up to forty percent (40%) of the spaces may be a minimum of 25' x 20'. The applicant has indicated that the majority of the spaces within the existing park are 30' wide by 50'-70' in depth, which exceed the minimum required.

Separation: Design of spaces shall be such that minimum space between recreational vehicles is ten feet (10'). The proposed spaces will have at least 10' of clearance between adjacent RVs or mobile homes.

Sanitary Facilities: Toilet facilities and potable water shall be provided in all campgrounds and in all recreational vehicle parks that allow tents or recreational vehicles which are not self-contained. The applicant is not proposing to have any tents or RVs that are not self-contained, so this standard does not apply.

Open Space: Common open space will be provided for each space in the park. This area shall be exclusive of all setback areas and shall be developed with turf and trees. The size of the open spaces shall be left to the discretion of the developer. There is currently open space in the northeast corner of the property as well as the southwest corner where the South Fork of the Palouse River traverses the subject property.

Refuse: Trash receptacles shall be provided, and shall be located no further than one hundred fifty feet (150') away from any recreational vehicle or campground space. Each RV space and mobile home space has a designated roll cart which is collected weekly to satisfy this requirement.

Streets: Internal private streets shall be a minimum of twenty-eight feet (28') (including a 20 foot surface and 4 foot shoulders) in width for two-way streets and twenty feet (20') (including a 12 foot surface and 4 foot shoulders) for one-way streets, with a minimum of three-quarters inch (3/4") processed rock, six inches (6") in depth. Minimum outside radius of all curves in streets shall be forty feet (40'). The existing mobile home park has approximately 30 foot wide paved internal streets which are designated as one-way, which exceed the street standards requirement. All corners in the loop meet the 40 foot outside radius requirement.

Utilities: All recreational vehicle parks and campgrounds with access to City sewer and/or water shall be served by such. The existing mobile home park is currently serviced off existing water and sewer mains that are connected to the City's mains. Each individual RV space will have connections to water and sewer.

RECOMMENDATIONS:

Staff recommends that the City of Moscow Planning and Zoning Commission approve the RV Park proposal for Palouse Hills Mobile Home Park at 913 Palouse River Drive and waive the final plan review.

Palouse Hills Mobile Home Community Application for RV Spaces

Property Address: 913 W. Palouse River Drive, Moscow, Idaho
Applicant: Dan Gruss, Manager, Palouse Hills Mobile Court, DMG Rentals
14246 SW 133rd Ave., Tigard, OR 97224
503-502-3433; dangruss@outlook.com

PROPERTY DESCRIPTION:

Palouse Hills Mobile Court is located at 913 West Palouse River Drive and consists of 3.65 acres including parcel RPM00100060030A.

A parcel of land located in Plot number 6 of Allen Tracts, described as follows:

Situated in the NE ¼ of Section 19, Township 39 North, Range 5 West, Boise Meridian, as shown by the recorded plat thereof.

ADDITIONAL DESCRIPTION;

Palouse Hills Mobile Court consists of one (1) loop. Please refer to the plat map for schematic configuration of the park. The loop is paved and approximately 30 feet wide. Parking is in front of and between trailers. Traffic direction is one way (see arrows on plat map for direction of traffic flow).

Latah Sanitation provides garbage services in the park and is billed monthly by the City of Moscow (City). Each resident has individual trash receptacles that are emptied weekly. Weekly garbage removal and city water and sewer services are included in the monthly lot rent.

There are 33 trailer pads in Palouse Hills Mobile Court. Pad dimensions in the park vary, but generally are 30 feet wide by 50'-70' deep. Currently there are 4 pads without a manufactured home on them. They are 22, 30, 31 and 32. Older trailers may be removed upon tenant turnover, however, and replaced with RV's or newer manufactured homes. These are the pads I would like to fill as soon as possible.

Along West Palouse River Road there is a fourplex that is adjacent to a storage building. Three quarters of the way around the loop is a single, stand-alone dwelling unit.

OBJECTIVE:

This is a request for permission from the City to allow recreational vehicles (RV's), newer manufactured homes and "tiny homes" to fill pads when and where feasible. This will increase the monthly cash flow for the park, allowing for the re-investment of capital to make the aesthetic and infrastructural improvements need to increase the quality of life for the tenants. In addition to a better return on investment, this will also help fill a critical demand for affordable housing in the City.

Due to the requirement to keep spacing between mobile homes a minimum of 10-feet apart, several pads are too short to hold anything but an RV. These spaces will be long-term RV locations. Currently,

spaces 6, 7, 8, 12, 22, 28, 30, 31, and 32 fit this category – others may fit this description upon final evaluation.

IMPROVEMENTS IN RECENT YEARS:

Over time the park had become associated with drug dealers, deferred maintenance needs, outdated electrical panels, excessive trash in empty lots, and rundown manufactured homes. In addition, Palouse Hills Mobile Court was the victim of Arson in September of 2018 where someone deliberately set fire to one of the homes that was vacant and under remodel, which caught two other homes on fire that were on both sides of the home. These homes were completely destroyed and are now apart of the pads with no manufactured home on them. We have advertised all the pads that are empty, to no avail because they are too short for newer manufactured homes.

Priority actions by DMG Rentals have been addressing these issues and making significant improvements. The park is looking better, is safer and problematic tenants have been removed/are currently being dealt with through the attorneys and the City judicial system. DMG Rentals has updated several homes and sold them via rent to own contracts that mature in less than five years.

SUMMARY:

Most of the current tenants have given me high praise for efforts being made to make their home lives better. The community is coming together and we have had annual spring clean up days and recently painted the exteriors of several trailers – the tenants provide the labor and DMG Rentals provides the materials.

Since being more actively involved in the management of the park since 2015, DMG Rentals has had excellent experiences with City personnel involved in various issues, the Police and Fire departments in dealing with tenant issues.

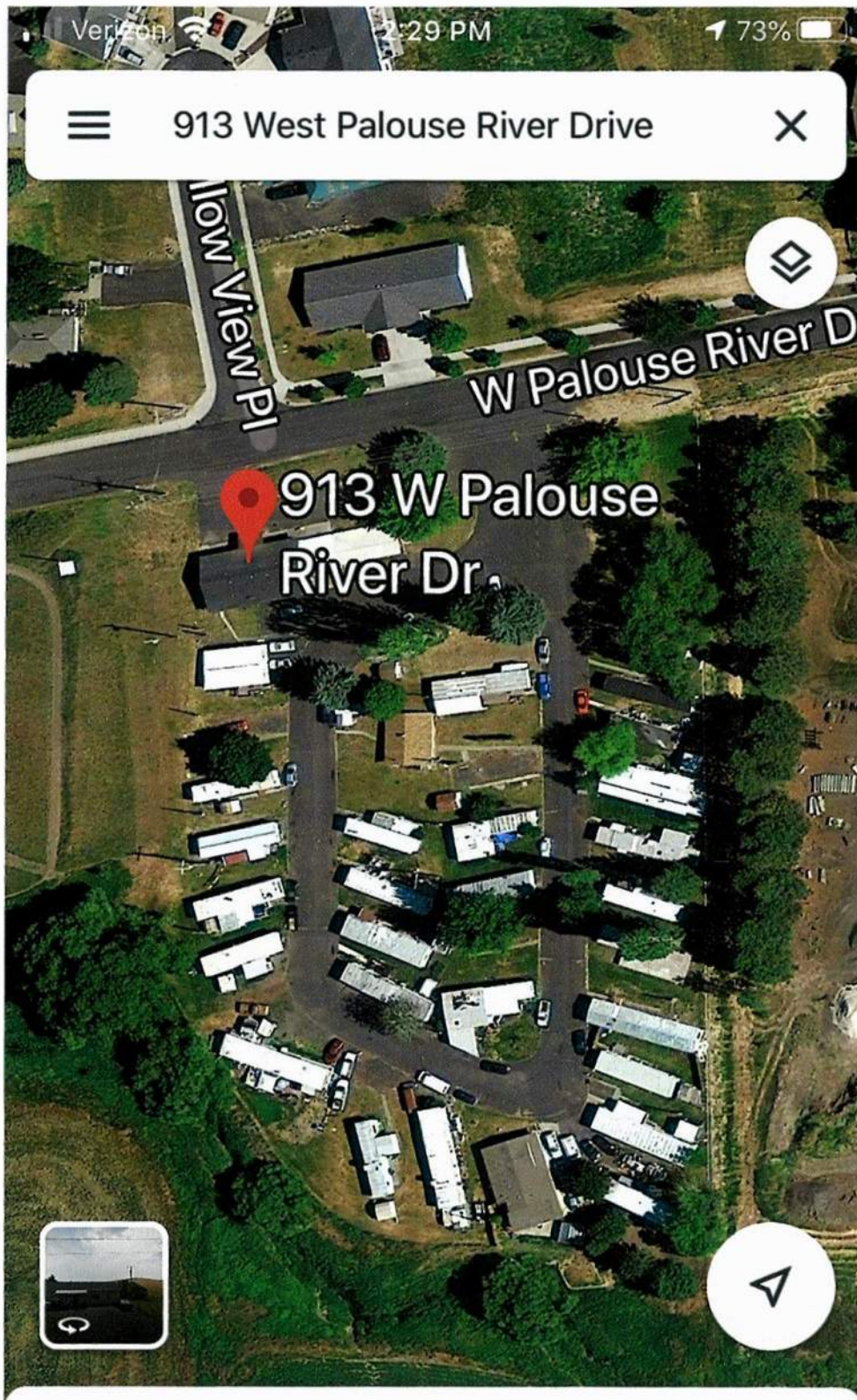
Thank you for considering the request for RV spaces throughout the park. If approved, this will greatly accelerate the efforts to continue to turn the park around and increase the quality of the neighborhood.

Sincerely,



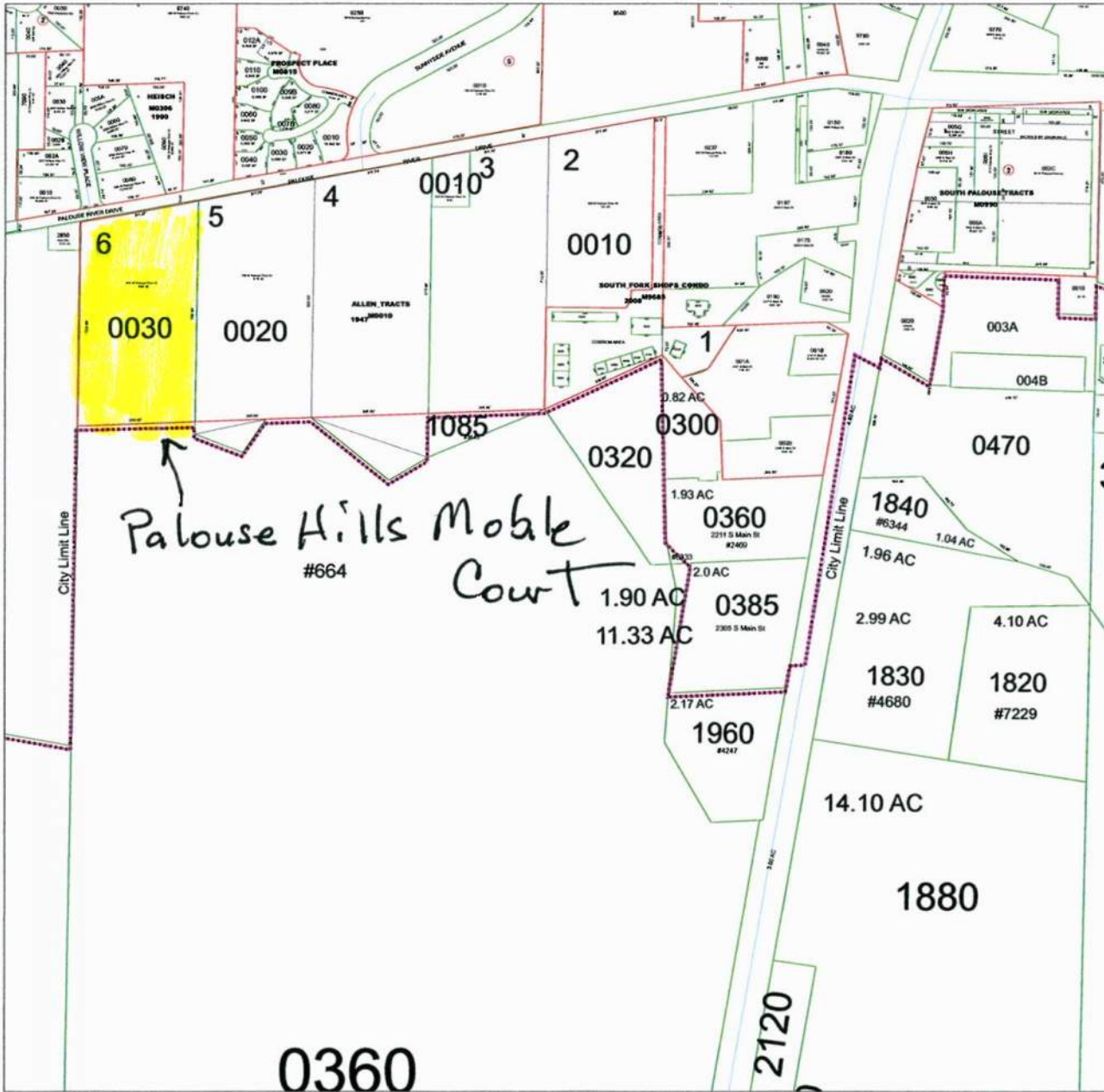
Dan Gruss, Owner/Operator
DMG Rentals
Palouse Hills Mobile Court



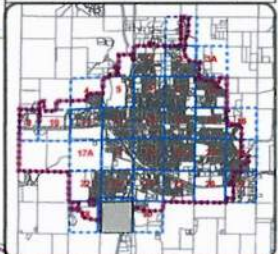


913 W Palouse River Dr

913 W Palouse River Dr, Moscow, ID 83843



**LATAH COUNTY ASSESSOR
OFFICIAL PLAT RECORD
(Moscow)**
T39NR05W SEC. 19 NE1/4



SUBDIVISION INDEX	
Allen Tracts	M 0010
Heisch	M 0396
Prospect Place	M 0815
South Palouse Tracts	M 0990
South Fork Shops Condo	M 9685

LEGEND

Legend	
	Subdivisions
	Parcels
	Railroads
	Streets
	Lot Lines

© Latah County Official Plat Record.
These maps are provided by The Latah County Assessor.
Due to the variability of sources used Latah County makes no claims or warranties regarding the data contained herein.

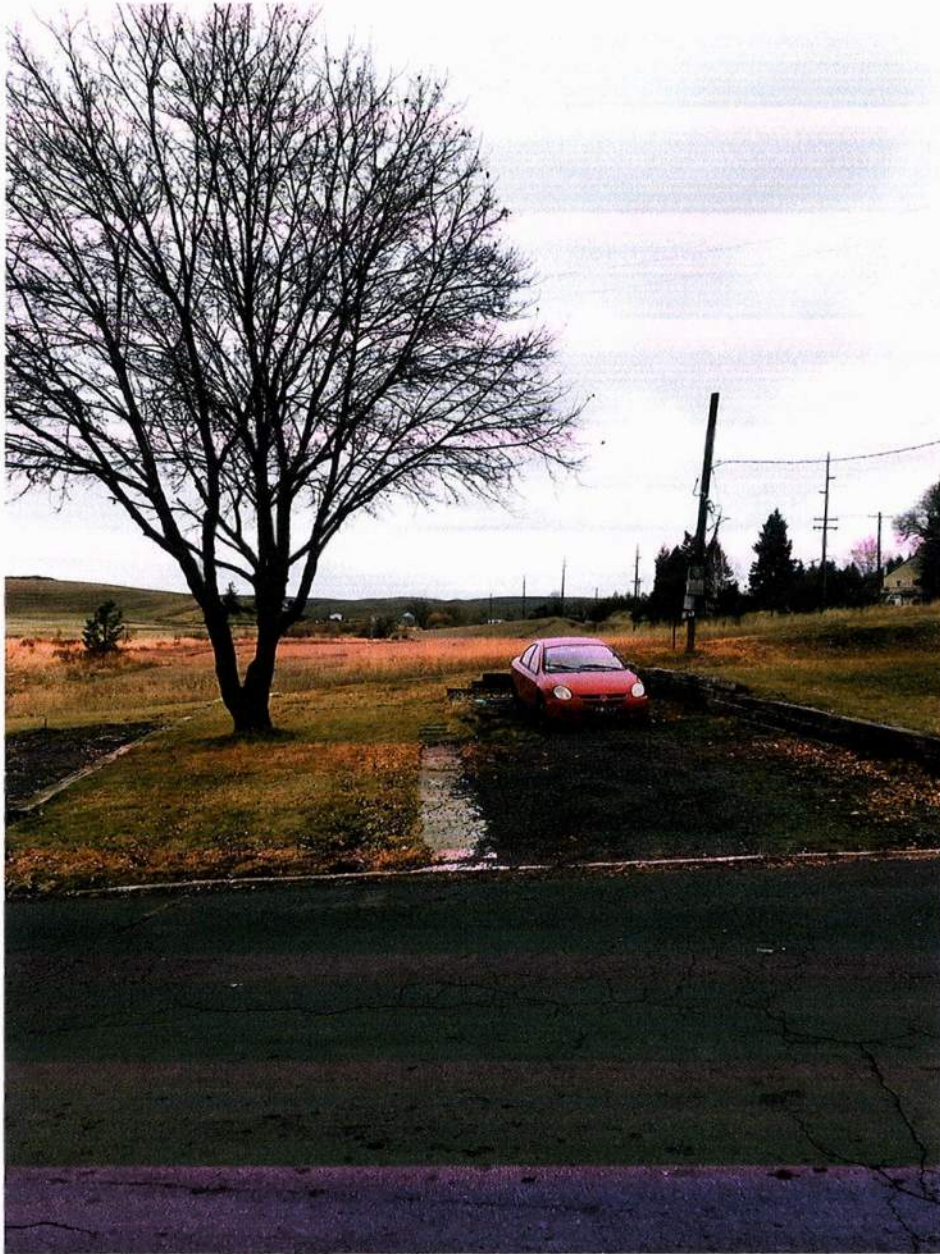
SCALE	SHEET NUMBER
1"=100'	M 29
PLOT DATE	
Aug 2018	
FILE NAME	
moscow_mapbook.mxd	



PHMC, Space 22



PHMC Road in front of spaces 30, 31, 32



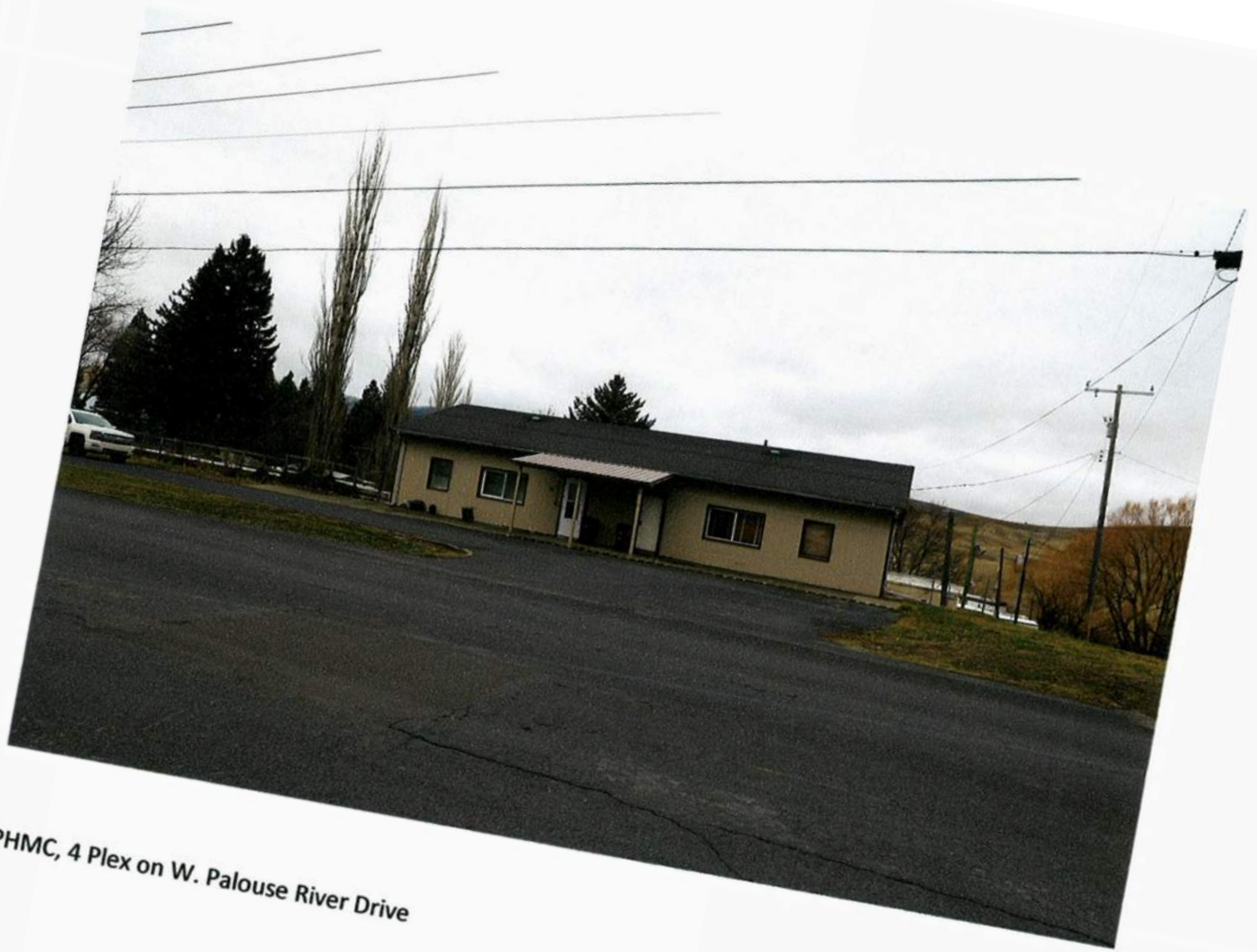
PHMC, Space 30



PHMC, Space 31



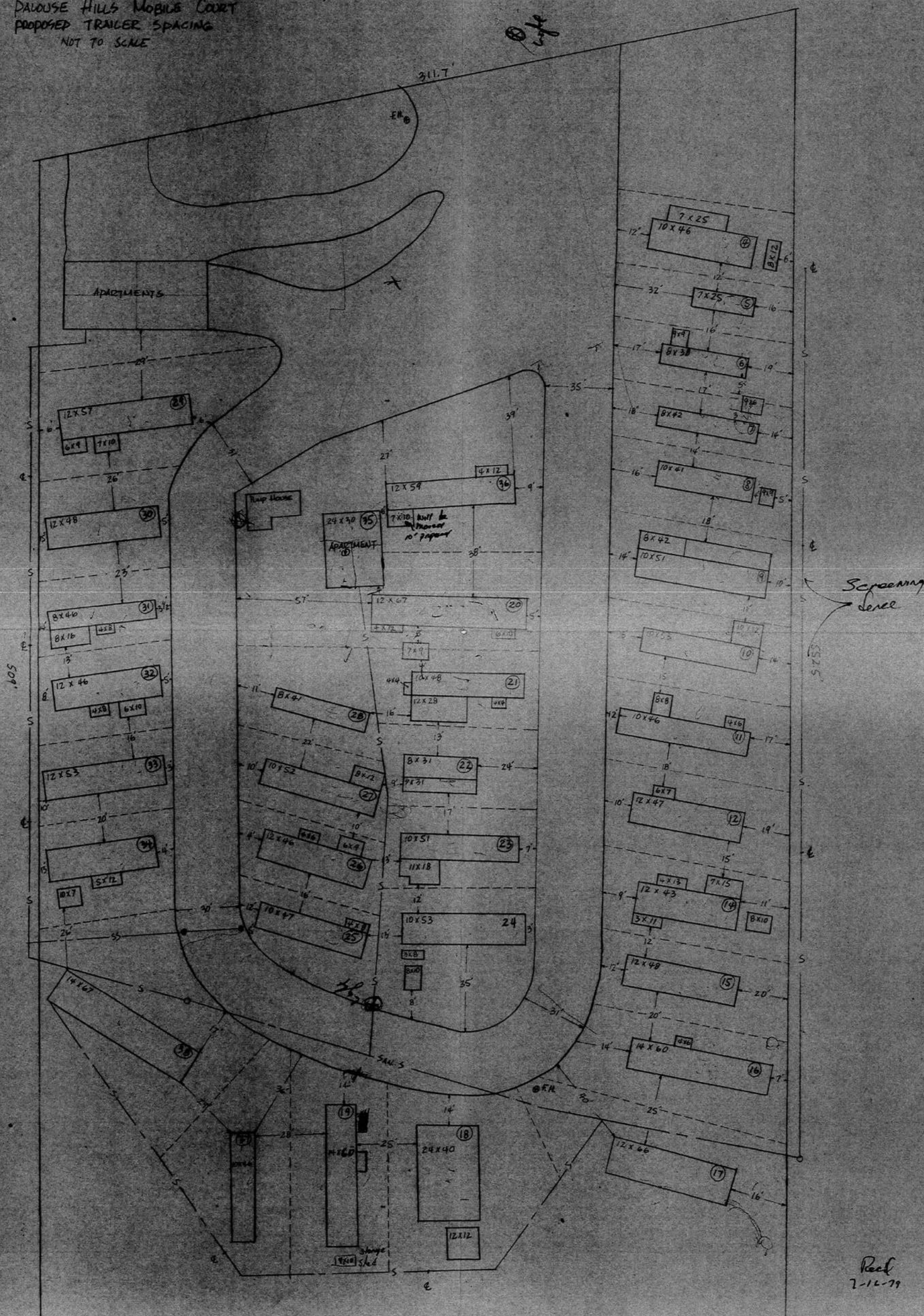
PHMC, Space 32



PHMC, 4 Plex on W. Palouse River Drive

DAVOUSE HILLS MOBILE COURT
 PROPOSED TRAILER SPACING
 NOT TO SCALE

33 TRAILERS



Reel
 7-12-79

COMMUNITY PLANNING AND DESIGN DEPARTMENT STAFF REPORT

HEARING DATE: May 13, 2020

GENERAL INFORMATION

Hearing Body: Planning and Zoning Commission

Subject: Application for a Recreational Vehicle (RV) Park at 713 Brent Drive - LUP2020-0007

Attachments:

1. Proposal Letter
2. Site Map
3. Topographical Map
4. Site Plan

Prepared by: Mike Ray, AICP

STAFF REVIEW

Proposal: The applicant, Robinson Mobile Home Court, is requesting the allowance of Recreational Vehicle (RV) spaces within the Bluebird Mobile Home Park which they own at 713 Brent Drive. There are approximately 23 sites within the Bluebird Mobile Home Park and there are a number of sites that are challenging to accommodate mobile homes, but could be suitable for RV spaces.

Site and Area Land Use: The subject property was first established as Loney's Trailer Court in the 1970's and has since been purchased by Robinson Mobile Home Park and renamed Bluebird Mobile Home Park within the last year. The subject property is currently 2.54 acres and contains a one-way looped access drive. There are currently 23 spaces within the park and vary between 30' wide by 50' - 70' deep.

Access to the subject property is provided via Brent Drive to the north and Morton Street to the south, which are both designated as local neighborhood streets. To the east of the subject property are numerous commercial uses which include Norm's Glass, Moscow Building Supply storage building, and a veterinary clinic. To the south is an electrical contractor and a woodworking business, and to the west is a self-storage facility. To the north along Brent Drive are residential single-family and two-family dwellings.



Vicinity Map

Zoning: The subject property and adjacent properties to the east are currently zoned Motor Business (MB). Properties to the east, west, and south are also zoned MB, while properties to the north are zoned Medium Density Residential (R-3).

According to the City of Moscow Zoning Code, the purpose of the MB Zoning District is to,

“The MB Zoning District is intended to provide for the location and grouping of compatible uses having similar operation as they involve enterprises which depend primarily on the transient motor vehicle-based trade. By concentrating such uses the City intends to increase public convenience when utilizing such services and, within the regulation, to allow such activities to render the maximum service.”

Recreational Vehicle (RV) parks are a permitted use within the MB Zoning District, but require review and approval by the Planning and Zoning Commission.



Existing Zoning

Comprehensive Plan: The subject property is designated as High-Density Residential upon the 2019 City of Moscow Comprehensive Plan Land Use Map. Surrounding properties to the west, south, and east are designated as Auto-Urban Commercial, while the residential properties along Brent Drive to the north are designated as Auto-Urban Residential.

According to the 2019 Comprehensive Plan, the High-Density Residential designated areas are,

“intended to allow the highest density residential development at common densities between 15 and 26 units per acre. Areas appropriate for this designation include those located adjacent to areas of higher activity where there is a range of uses. High-Density Residential areas should be located adjacent or in close proximity to designated arterial or collector streets. New areas of high-density residential development should be located near mixed-use activity areas and corridors that accommodate multi-modal transportation. Lands designated as Urban Residential are generally appropriately zoned Medium Density Residential (R3), Multiple Family Residential (R-4), or Residential Office (RO).”

Access, Streets, Traffic: The subject property has existing access via Brent Drive which is developed as a 30-foot-wide paved street with curbing and no sidewalks. Morton Street to the south is a gravel street with no curbing or sidewalks.

RV Park Standards:

Size of RV Spaces: The minimum space size within an RV Park shall be 40' x 20'; however, up to forty percent (40%) of the spaces may be a minimum of 25' x 20'. The applicant has indicated that the majority of the spaces within the existing park are 30' wide by 50'-70' in depth, which exceed the minimum required.

Separation: Design of spaces shall be such that minimum space between recreational vehicles is ten feet (10'). The proposed spaces will have at least 10' of clearance between adjacent RVs or mobile homes.

Sanitary Facilities: Toilet facilities and potable water shall be provided in all campgrounds and in all recreational vehicle parks that allow tents or recreational vehicles which are not self-contained. The applicant is not proposing to have any tents or RVs that are not self-contained, so this standard does not apply.

Open Space: Common open space will be provided for each space in the park. This area shall be exclusive of all setback areas and shall be developed with turf and trees. The size of the open spaces shall be left to the discretion of the developer. There are minimal common open space areas, as this is an existing mobile home park that is approximately 50 years old. Space is constrained on the site and there are virtually no opportunities to create additional open space.

Refuse: Trash receptacles shall be provided, and shall be located no further than one hundred fifty feet (150') away from any recreational vehicle or campground space. Each RV space and mobile home space has a designated roll cart which is collected weekly to satisfy this requirement.

Streets: Internal private streets shall be a minimum of twenty-eight feet (28') (including a 20 foot surface and 4 foot shoulders) in width for two-way streets and twenty feet (20') (including a 12 foot surface and 4 foot shoulders) for one-way streets, with a minimum of three-quarters inch (3/4") processed rock, six inches (6") in depth. Minimum outside radius of all curves in streets shall be forty feet (40'). The existing mobile home park has a 12-foot-wide one-way paved internal street with gravel shoulders on both sides, which meets the street standards requirement. All corners in the loop meet the 40 foot outside radius requirement.

Utilities: All recreational vehicle parks and campgrounds with access to City sewer and/or water shall be served by such. The existing mobile home park is currently serviced off existing water and sewer mains that are connected to the City's mains. Each individual RV space will have connections to water and sewer.

RECOMMENDATIONS:

Staff recommends that the City of Moscow Planning and Zoning Commission approve the RV Park proposal for Bluebird Mobile Home Park at 713 Brent Drive and waive the final plan review.

RECEIVED

MAR 03 2020

City of Moscow

pd 3/3/20

LUP 2020-0007

Robinson Mobile Home Court

901 N Almon St

Moscow, ID 83843

(208) 882-5429

robinsonmobilehomepark@gmail.com

March ³~~6~~, 2020

City Council, City of Moscow,

As many of you know Loney Pine trailer court has been purchased and renamed Bluebird Mobile Home Park. We are located at 713 Brent Drive, Moscow. We are asking for permission and seeking a permit to allow us to park RV's in this park.

It is not our intent to have overnight RV parking, rather to establish an affordable alternative house options. One that could benefit college students and others who are seeking a place to place an RV for a few months, to a year, as they need.

We have removed some homes in the park that were vacant, and did not meet the standards we would like in our park. We have been actively working on improving the image of the park and the community. We have torn down some homes that were not economical to repair. We believe these spaces would work well within the community to provide a safe and convenient location for RV's to be placed.

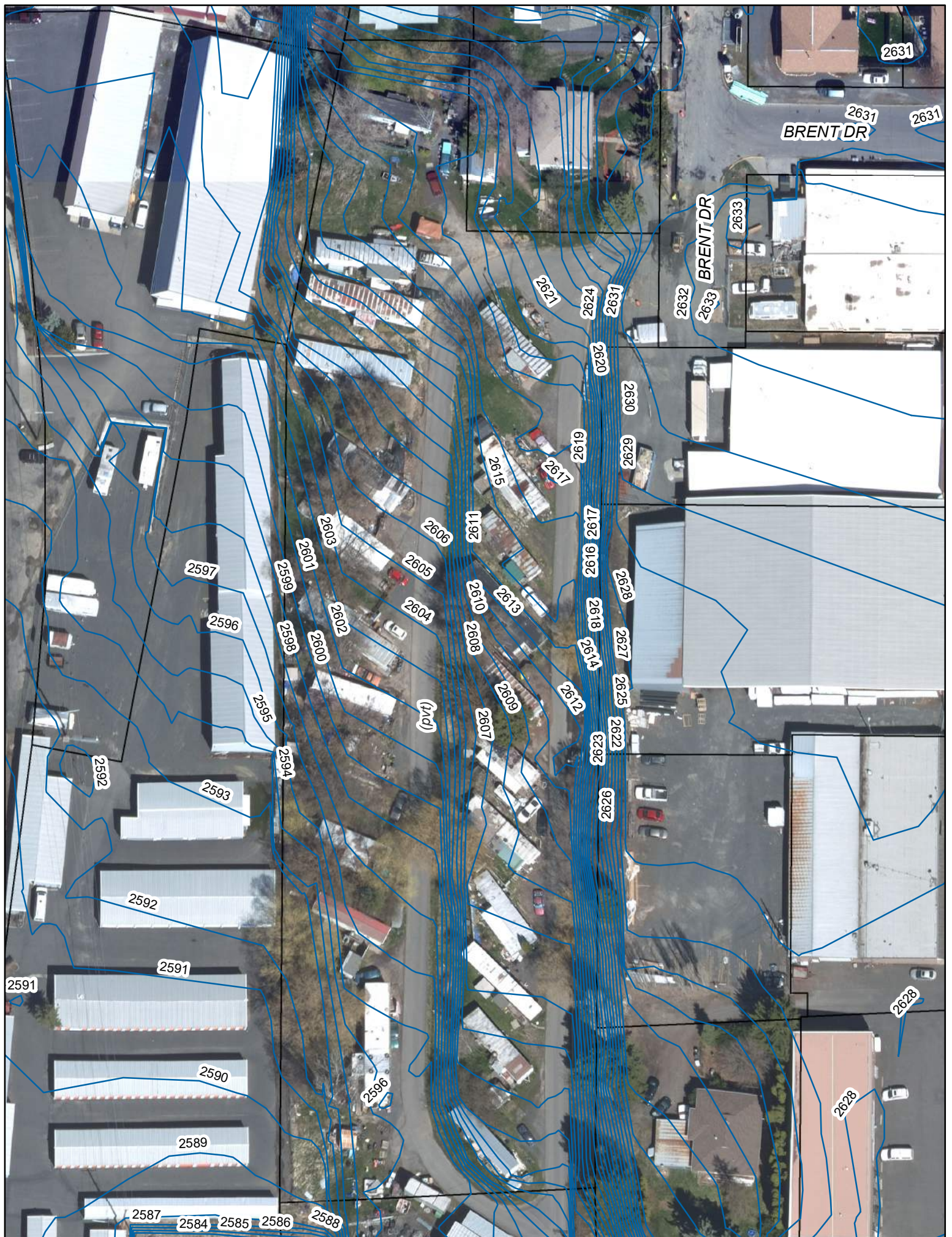
Please consider our request and allow us to make this option available to those in need.

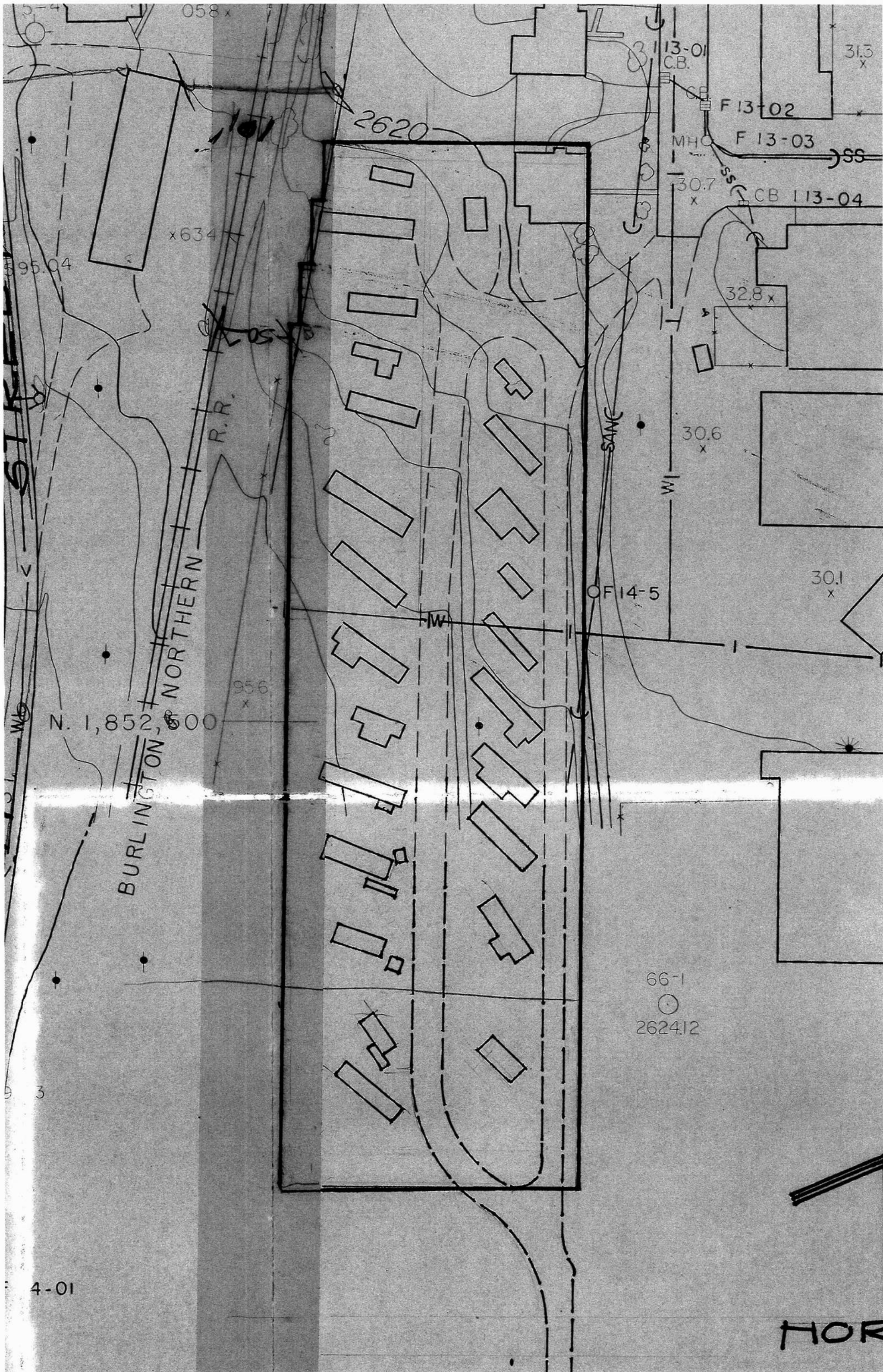
Yours,



Mike Bowers, Community Manager







BURLINGTON

NORTHERN

R.R.

OF14-5

F13-02

F13-03

CB 113-04

30.6 x

30.1 x

32.8 x

30.7 x

31.3 x

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66-1
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HOR

CITY OF MOSCOW COMMUNITY PLANNING AND DESIGN DEPARTMENT DEVELOPMENT REVIEW REPORT

REVIEW DATE: May 13, 2020

GENERAL INFORMATION

Subject: Moscow Urban Renewal Agency request for redevelopment proposals for a parcel located at the southwest corner of the intersection of 6th Street and Jackson Street in Moscow, Idaho.

Applicant: Flatiron Development – Roderick Olps

Project Description: Moscow Flatiron LLC is proposing a mixed-use development which includes a 3-story 29,000 sf building, a 2-story 2,300 sf building, a 25 space parking lot, rooftop with sky bridge, and an extension of Hello-Walk from the northeast corner of the property. The building is proposed to contain four commercial tenant spaces of 5,035 sf, 826 sf, 492 sf, and 1,105 sf on the first floor. The second floor is dedicated to a cooperative work area with 17 private offices and a conference room as well as a sky a bridge that connects to the smaller two-story building. The third floor has 12 one-bedroom/studio apartments and a space for a potential restaurant with rooftop patio.

The smaller two-story building has a 790 sf commercial space on the first and second floors, with a bathroom on the first floor. The second floor connects to the three-story building via the sky bridge.

Attachments:

1. Cover Sheet and Conceptual Images
2. Architectural Site Plan
3. Structural Layout
4. Ground Floor Plan
5. Second Floor Plan
6. Third Floor Plan
7. Roof Plan
8. Building Elevations
9. Civil Site and Landscape Plan

Prepared By: Mike Ray, AICP - Planning Manager

STAFF REVIEW

Summary: The proposed Flatiron Development complies with all of the requirements within the Legacy Crossing Overlay Design Guidelines document. The building placement and building height are all within the parameters dictated by the design guidelines. The proposed building complies with all of the building façade requirements which include front entry locations, façade design, the proportion of transparent window openings, projecting façade elements such as awnings, and building materials. The placement of off-street parking and the landscaping surrounding the parking lot exceed the surface parking requirements of the code and meet the bicycle parking requirements. With the inclusion of the parking lot and Hello-Walk stormwater collection systems, the stormwater management provisions are satisfied by the proposal. Hello-Walk has been extended in accordance with the design guidelines and public art has been proposed in the existing circular plaza that serves as the entrance to Hello-Walk.

Urban Mixed Commercial (UMC) Zoning District:

Proposed Use	Permitted			Notes
	Yes	No	CUP	
Three-Story Building				
Commercial tenant spaces (first floor)	Y			Commercial uses that end up going into the tenant spaces will need to meet the allowed uses of the Urban Mixed Commercial (UMC) Zone.
Cooperative Office Space (second)	Y			
Studio Apartments (third floor)	Y			
Restaurant Space (third floor)	Y			
Two-Story Building				
Commercial tenant space	Y			
Commercial tenant space	Y			

Compliance Symbols:

Y – Yes NIP – No Information Provided
N – No NA – Not Applicable
P – Partial

Legacy Crossing Overlay (LCO) Design Guidelines

Criteria	Compliance	Notes
Pedestrian Corridor Facilities		
Continuation of Hello-Walk Corridor	Y	The proposed extension of Hello-Walk follows the location which is shown within the design guidelines.
Compliance with Hello-Walk Corridor Detail	Y	The proposed Hello-Walk detail is consistent with the detail that is shown within the design guidelines.
Building Placement		
Located within 5' of property lines along Jackson and Sixth Street	Y	Building setbacks from the build-to-line at intersection corners that are necessary to preserve visual sight distances are not to be counted against the maximum setback percentage and should be accomplished via angled or other building corner treatments to provide adequate motorist and pedestrian visibility. The proposed building corner treatment at Sixth and Jackson meets this sight distance provision.
20% of total building frontage length may be set back a maximum of 15' to accommodate façade articulation and public space amenities	Y	Both buildings meet the 5' maximum setback requirement.
Building Height		
Minimum height is 20' or two stories	Y	One building is three stories and the other is two stories.
Maximum height is 60' or five stories	Y	Proposal does not exceed this limitation.
General Building Façade Design		
Front façade shall include the main entrance and a number of transparent window openings	Y	Main entry locations are located on Sixth Street and the intersection of Sixth and also along the Hello-Walk corridor. Large transparent windows are located on the entire length of the building.

Blank walls on all facades that front a park, street, plaza, or other public spaces should be discouraged	Y	The same façade is continued on all sides of the buildings.
On all facades, a clear visual distinction between each floor should be provided	Y	Windows and vertical building treatments create a clear distinction between floors.
Facades should include both solid surfaces and window openings to avoid the creation of blank walls and/or glass curtain walls. Window openings should be balanced to avoid large blank wall surfaces on the façade	Y	The façade is evenly balanced between large windows and solid surfaces.
Rear and side facades should be designed with similar architectural elements, materials, and colors as the front façade. However, the design of side and rear facades may be simpler, and more casual in nature	Y	The rear and side facades are equivalent to the front façade.
Window Openings		
Fifty (50%) to eighty five (85%) percent of ground-floor façades that front public streets and pedestrian ways should be occupied by windows and/or entry doors.	Y	Large transparent windows and doors occupy approximately 50% of the front facades (N, NE, E).
Windows and doors should utilize clear transparent glass in order to provide clear views of storefront displays from the street and to allow natural surveillance of the street and adjacent outdoor spaces.	Y	Transparent glass is proposed to be used for the windows and doors.
Mirror and tinted glass should be discouraged. Moderately tinted glass for energy conservation should be allowed.	Y	There is no proposed mirror or tinted glass.
For all floors above the ground floor, windows should occupy at least twenty (20%)	Y	The second floor windows occupy approximately 40% of the total wall area.

of the total wall areas and should have a vertical orientation and proportion.		
Projecting Façade Elements		
Projecting façade elements should be in keeping with the historic architecture within the community including awnings and marquees.	Y	Heavy roof cornices, brick masonry, solid concrete, and metal awnings are consistent with historical architecture within the downtown area.
Where buildings are adjacent to public streets or pedestrian ways, at least 75 percent of the façade width should contain projecting façade elements that provide shade and shelter to pedestrians	Y	Approximately 75% of the façade widths adjacent to public streets or pedestrian ways contain metal awnings.
Façade Articulation		
Street facing building facades, as well as all facades that front a plaza or pedestrian way should be articulated to improve the quality of the design. Appropriate methods are described in Section 6.D.13	Y	The proposed building façades are articulated by increasing and varying the window openings; recessing windows and entryways; providing vertical and horizontal expression lines between the floors and around windows and doors; and incorporating heavy roof cornices.
Building Materials		
Appropriate building materials that are encouraged on facades are listed within Section 6.D.14	Y	Proposed building materials include brick masonry, metal awnings, metal windows and doors, and brick cornices
Inappropriate building materials that are discouraged on facades are listed within Section 6.D.15	Y	No inappropriate building materials are proposed to be used.
Painted building surfaces should have a matte or low-gloss finish but trim work may have a glossy finish.	Y	Only painted surfaces will be cornices which appear to have a low-gloss finish.

Required Off-Street Parking		
Vehicular parking should be required per the standards within Section 7.B.1	Y	25 parking spaces are provided and 8 are required.
Off-Street Parking Placement		
Off-street parking facilities should not be located between structures and adjacent public right-of-ways or pedestrian corridors	Y	The building and Hello-Walk corridor are located between the public right-of-way and off-street parking facility.
Off-street parking should be located underneath, behind, or to the side of all principle structures, or within parking structures	Y	Off-street parking facilities are located behind and to the side of the proposed development.
Other than public parking facilities, surface parking lots should be minimized in order to reduce the detrimental impacts of large surface parking lots within the LCO area	Y	The provided parking is consistent with what the parking requirement is.
Where podium parking is provided it should be screened from public view by the building with habitable space or other building features	NA	
Off-Street Parking Joint Use Provisions		
Shared joint use of off-street parking facilities among multiple properties is encouraged and may be approved by the approving body.	NA	
Off-Street Surface Parking Landscape Guidelines		
Surface parking areas should include one canopy tree placed within or around the	Y	5 canopy trees are required, 10 provided.

perimeter of the parking area for each five (5) stalls provided		
Off-street surface parking areas should be screened from adjacent public streets or pedestrian corridors by a five (5) foot wide landscape strip with shrubs or similar plantings that will provide adequate visual screening of the area at maturity and planted four (4) feet on center. The landscape strip may be reduced to three (3) feet in width with the inclusion of a fence or wall used for screening purposes.	Y	Landscape buffers that are adjacent to public streets meet the 5' width requirement and contain trees and shrubs. The remainder of the parking lot includes a 3' exterior buffer with shrubs every 6' and trees ever 40'.
Parking Structure Design Guidelines		
The exterior design of parking structures should minimize its visual identity as parking through façade articulation, window openings and variations in color, material and/or texture	NA	
Where possible, parking structures should be placed in a manner to minimize interruption of street frontage and screened from view by habitable space of the uses that they serve	NA	
Where parking structures are located adjacent to public streets or pedestrian areas, the exterior of the parking structure should include architectural detail such as decorative façade features, planters, and storefronts to minimize the appearance of the parking structure to motorists and pedestrians	NA	

Key Public Spaces		
Developments within the LCO area should incorporate public open spaces and features (such as plazas, fountains, seating and gathering areas and pocket parks) as key focal points within the LCO area as identified within Figure 8.1	Y	The proposal includes a larger plaza space that has already been constructed at the intersection of Jackson and Sixth Street which leads into the Hello-Walk corridor.
Public Art Integration and Key Installation Locations		
Public art incorporation within building facades, street furnishings, public installations and other development components are highly encouraged	Y	The applicant has proposed a piece of public art near the Jackson and Sixth Street intersection.
Developments should incorporate public art installations within all proposed key public spaces and the identified installation locations identified in Figure 8.1	Y	The applicant has proposed a piece of public art within the key public space which has been identified at the southwest corner of Sixth and Jackson.
Stormwater Management		
To the greatest extent possible all developments within the LCO area should utilize best management practices to capture, detain, reuse, treat and otherwise control and reduce stormwater runoff from the development site. Such management plans should include items such as pervious pavement materials, bio-swales, green roofs, rainwater harvesting, and other such practices	Y	The proposal includes stormwater storage systems in the parking lots and the Hello-Walk corridor, which utilizes pavedrains and permeable concrete pavers.
All developments within the LCO area should integrate low impact stormwater management practices in	Y	

addition to meeting all other stormwater requirements and provisions of Moscow City Code		
Public Transit Facilities		
All developments within the LCO area should assess the need for transit facilities necessary to serve the development including the location of public transit stops, benches, shelters and signage.	NA	
Bicycle Parking Facilities		
Developments within the LCO area should provide bicycle parking for each use of a lot or structure in accordance with the standards found within Section 9.C.1	Y	24 bicycle parking spaces are required and 24 are shown on the site plan.
Each bicycle parking space shall be sufficient to accommodate a bicycle at least six (6) feet in length and two feet wide, and shall be provided with some form of stable frame permanently anchored to a foundation to which a bicycle frame and both wheels may be conveniently secured using a chain and padlock, locker, or other storage facilities which are convenient for storage and are reasonably secure from theft and vandalism.	Y	Bicycle spaces of the required dimensions are shown on the site plan.
The separation of the bicycle parking spaces and the amount of corridor space must be adequate for convenient access to every space when the parking facility is full.	Y	
When automobile parking spaces are provided in a	NA	

structure, all required bicycle spaces shall be located inside that structure or should be located in other areas protected from the weather. Bicycle parking spaces in parking structures should be clearly marked as such and should be separated from auto parking by some form of barrier to minimize the possibility of damage from a vehicle.		
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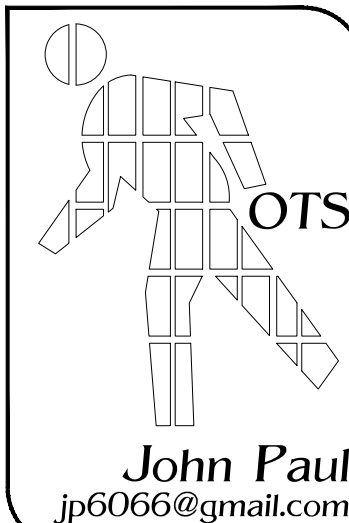
6th & Jackson
Moscow, Idaho



Northeast Perspective

[illegible]

Moscow Flatiron Development
for Moscow Flatiron LLC



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Date: 03/12/20

Sheet Contents:
Prelim Docs

A-0



Southwest Perspective



Southeast Perspective

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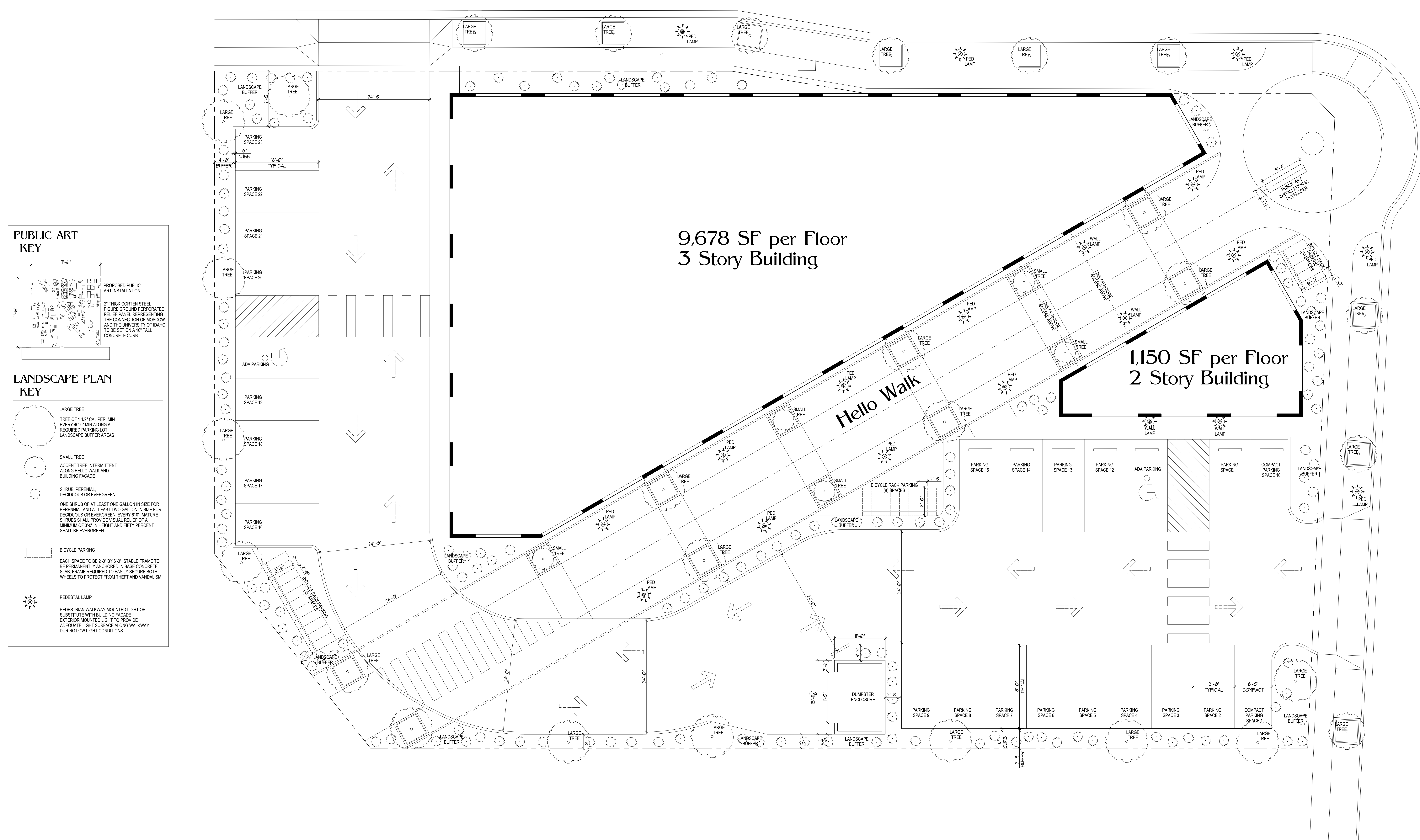
6th & Jackson
 Moscow, Idaho

Moscow Flatiron Development
 for Moscow Flatiron LLC

OTS
 John Paul
 jp6066@gmail.com

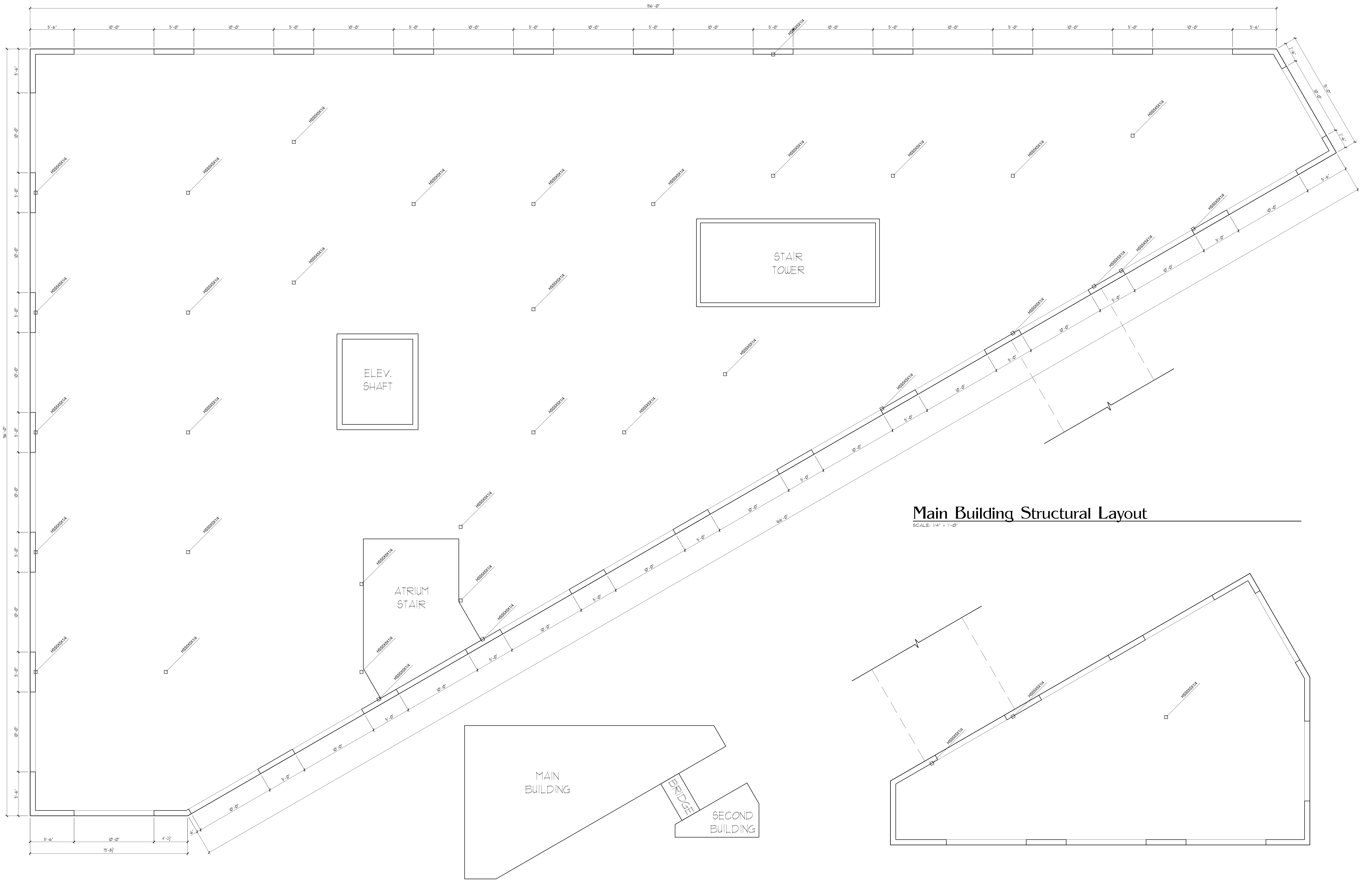
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 Prelim Site Plan

Jackson Street



Proposed Site Landscape Plan





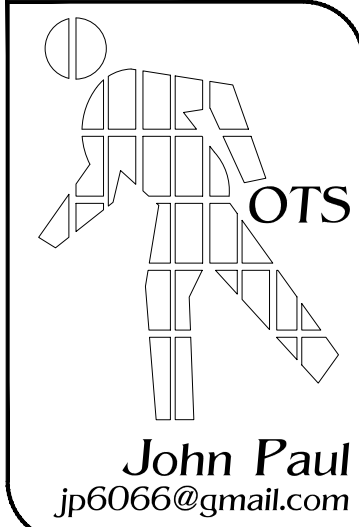
Key Plan

Secondary Building Structural Layout

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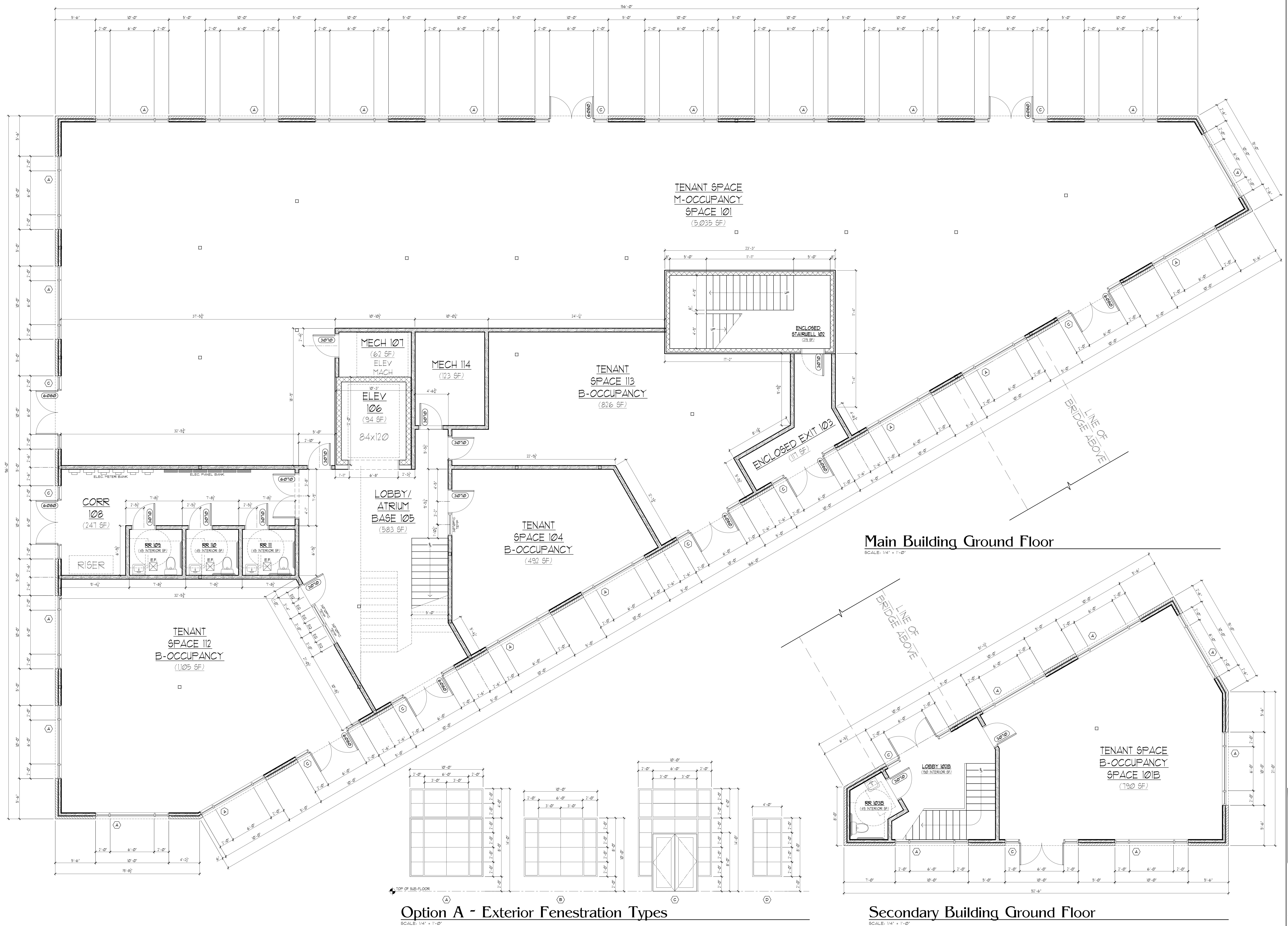
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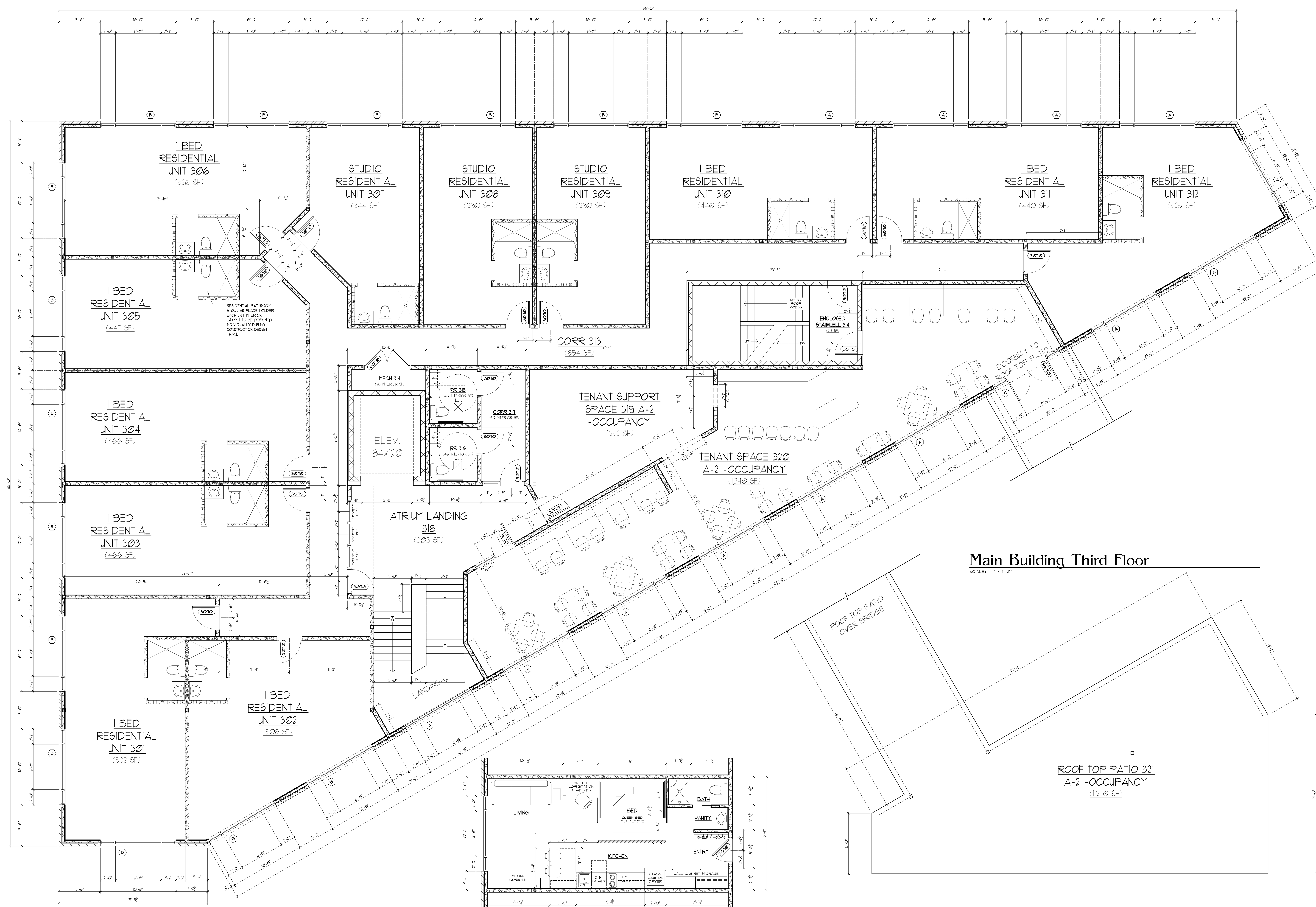
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Moscow, Idaho

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jp6066@gmail.com

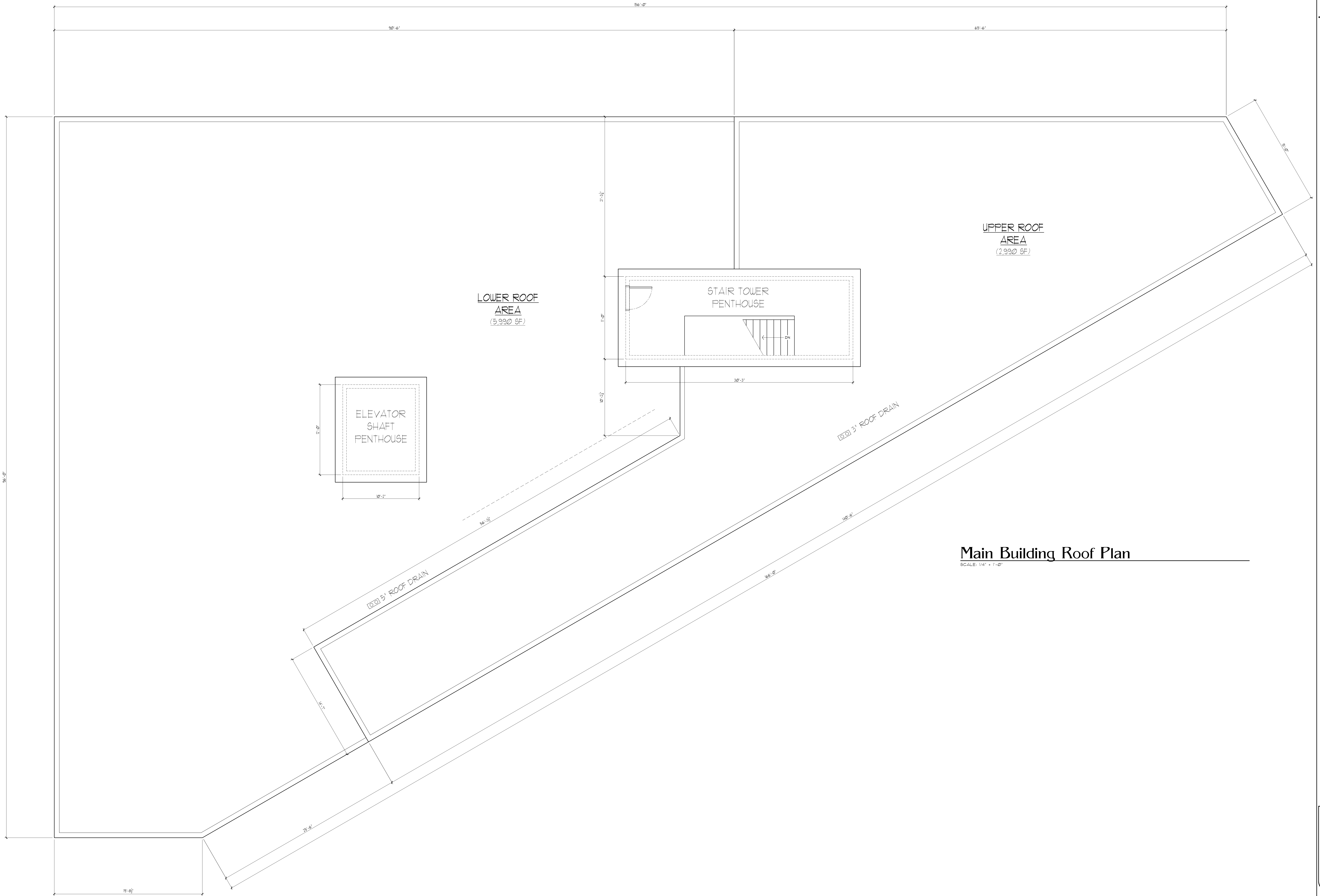
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A-4



Typical 3rd Floor Residential Unit

Secondary Building Roof Top Patio



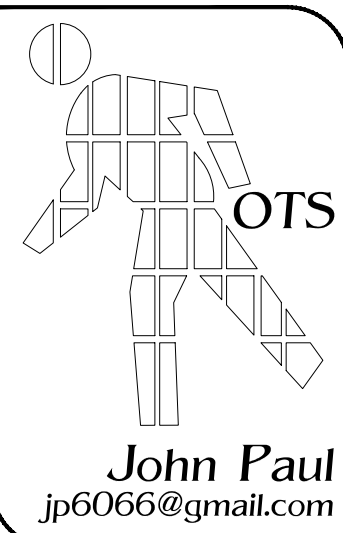
Main Building Roof Plan

SCALE: 1/4" = 1'-0"

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6th & Jackson
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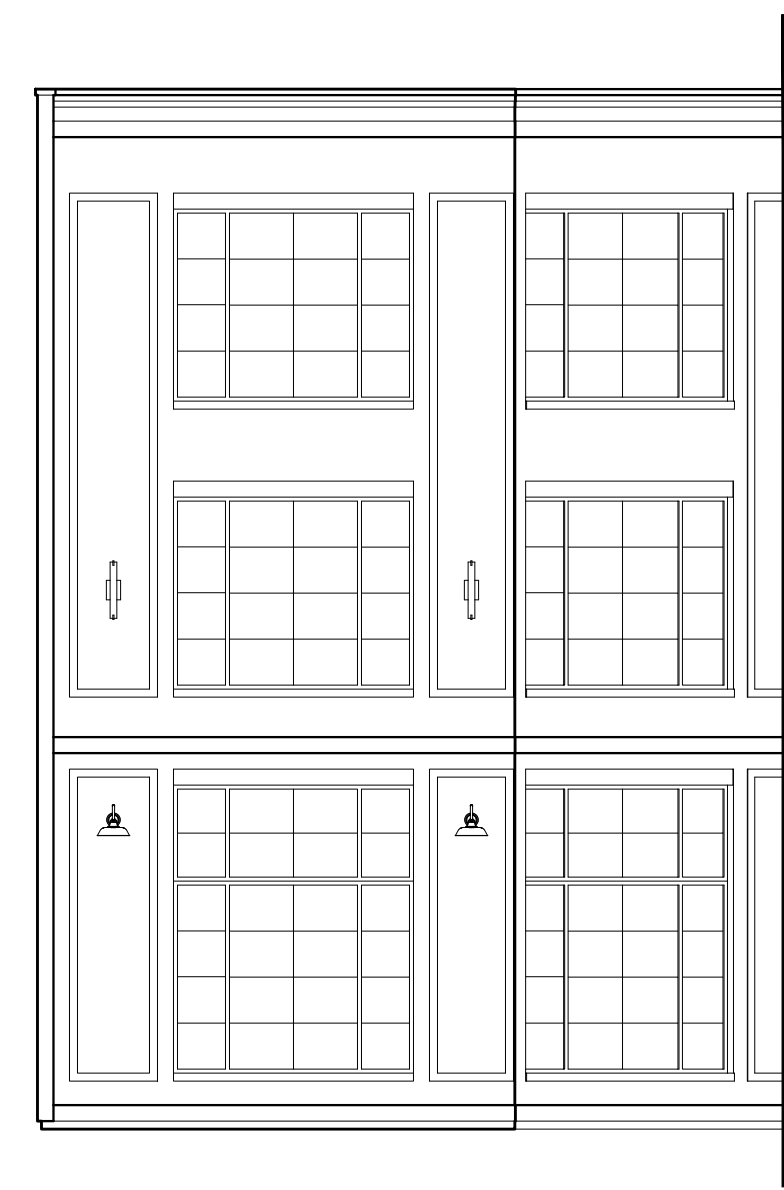
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Main Building West Elevation



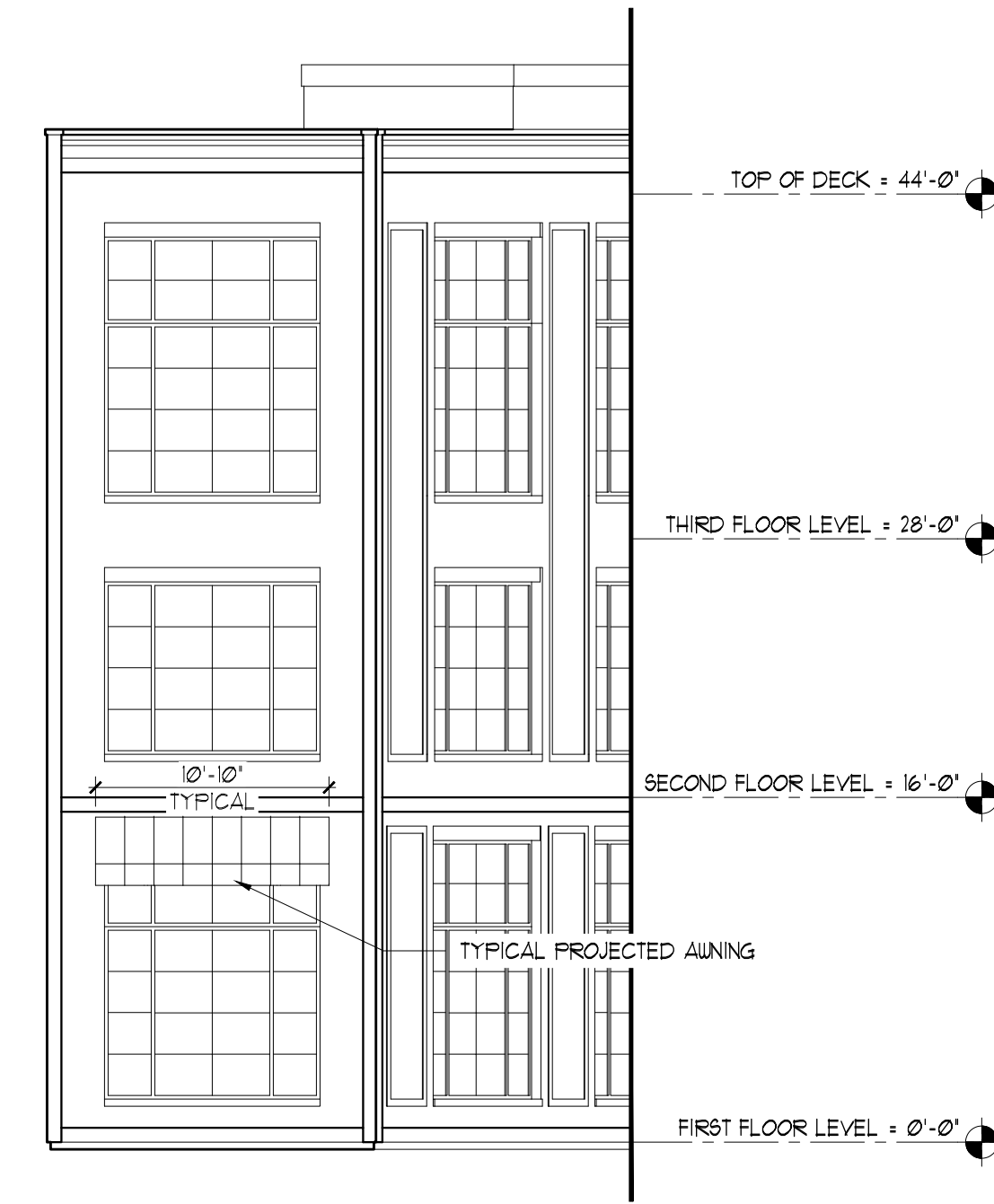
Main Building North Elevation



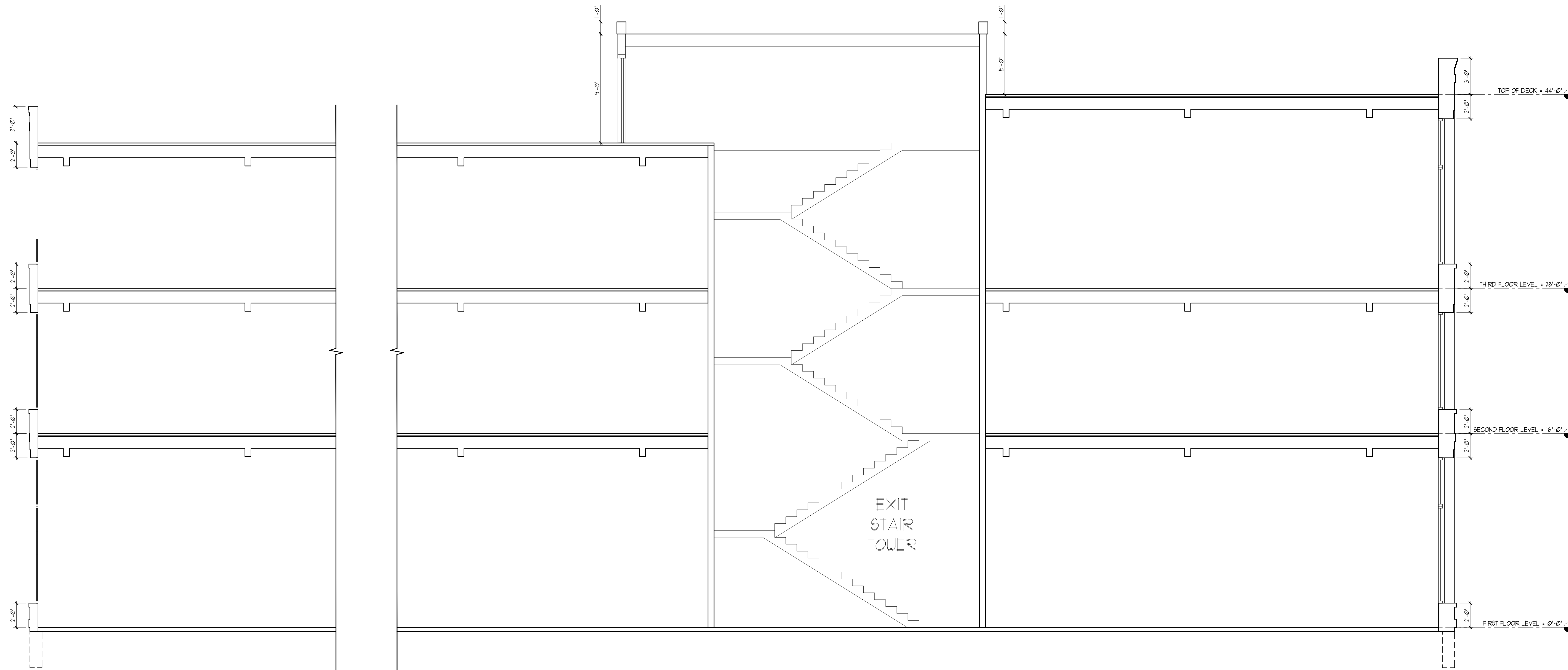
Main Bldg S Ele



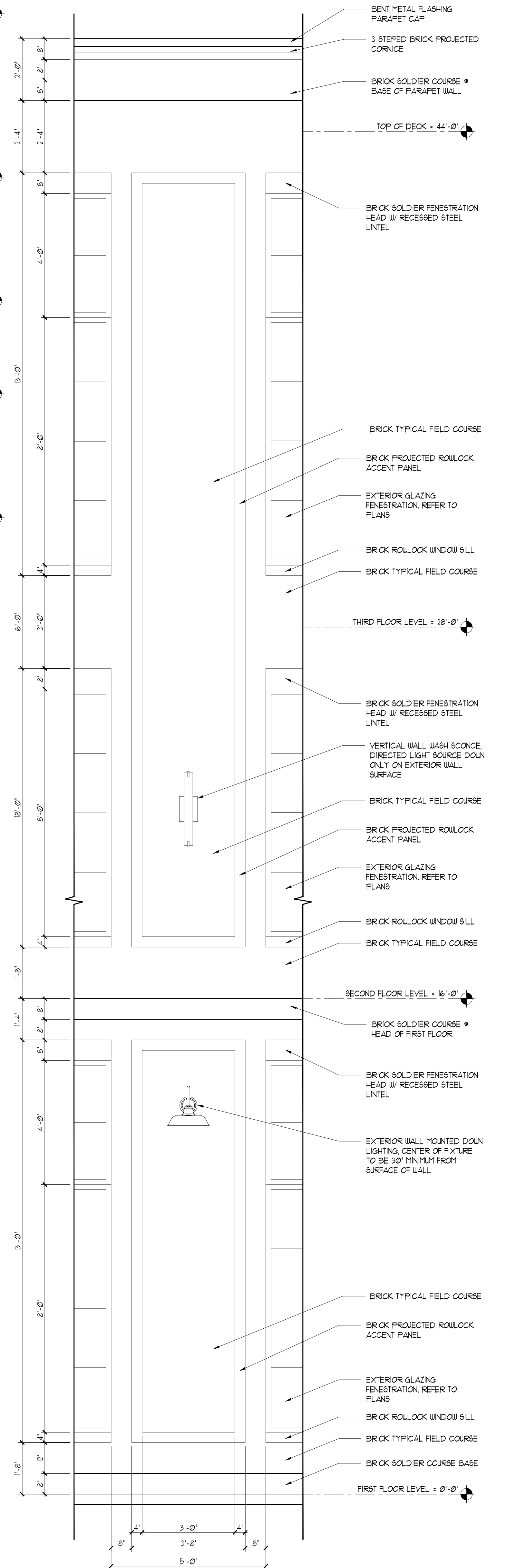
Southeast Elevation



Main Bldg NE Ele.



Main Building East/West Section @ Stair Tower

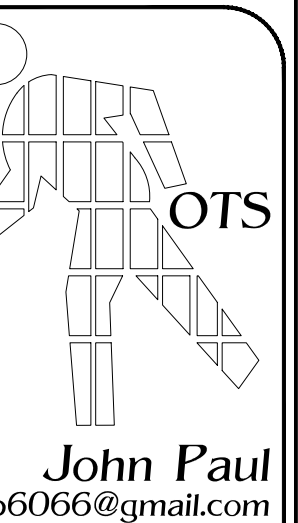


Typical Brick Facade Layout

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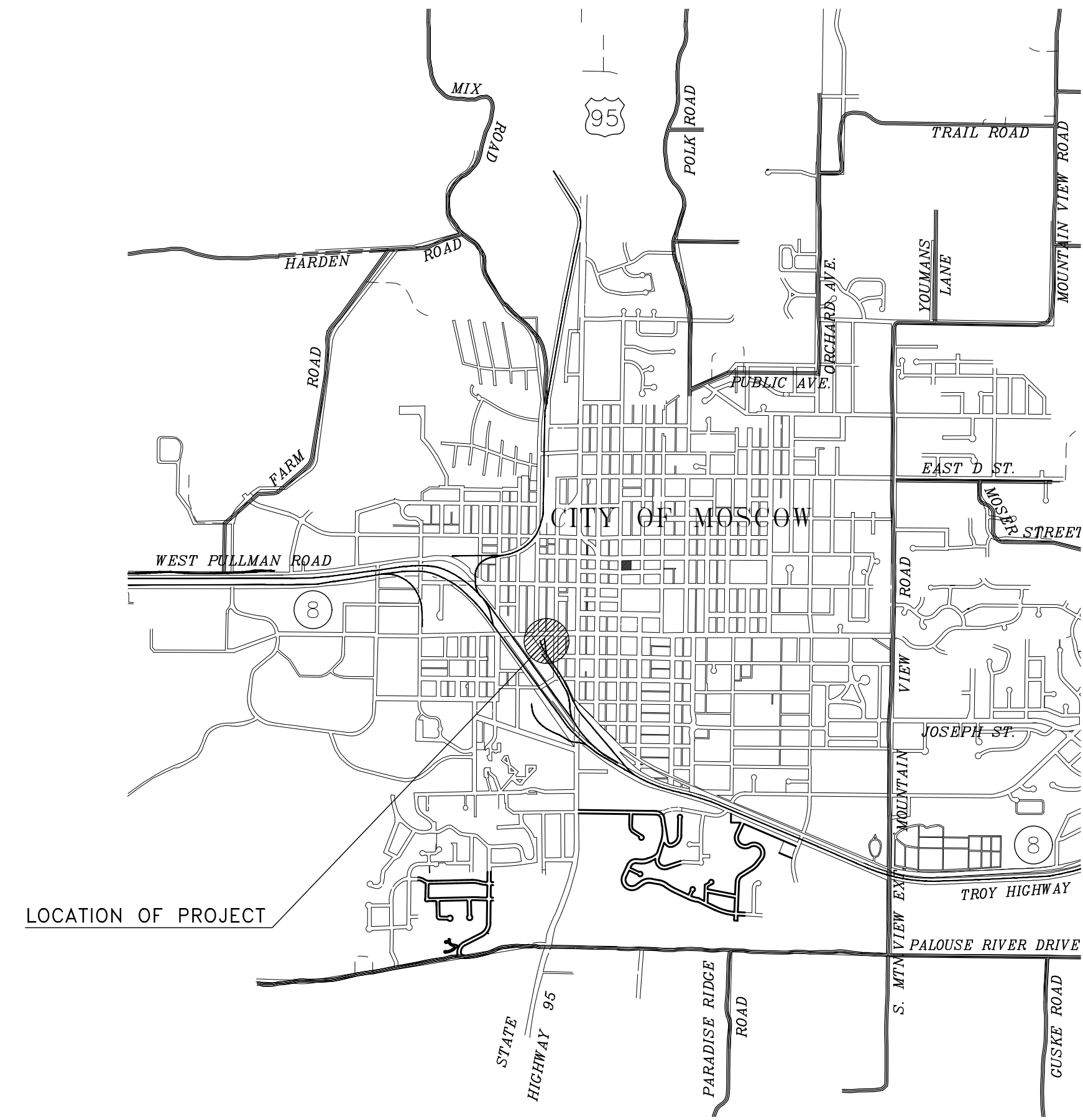
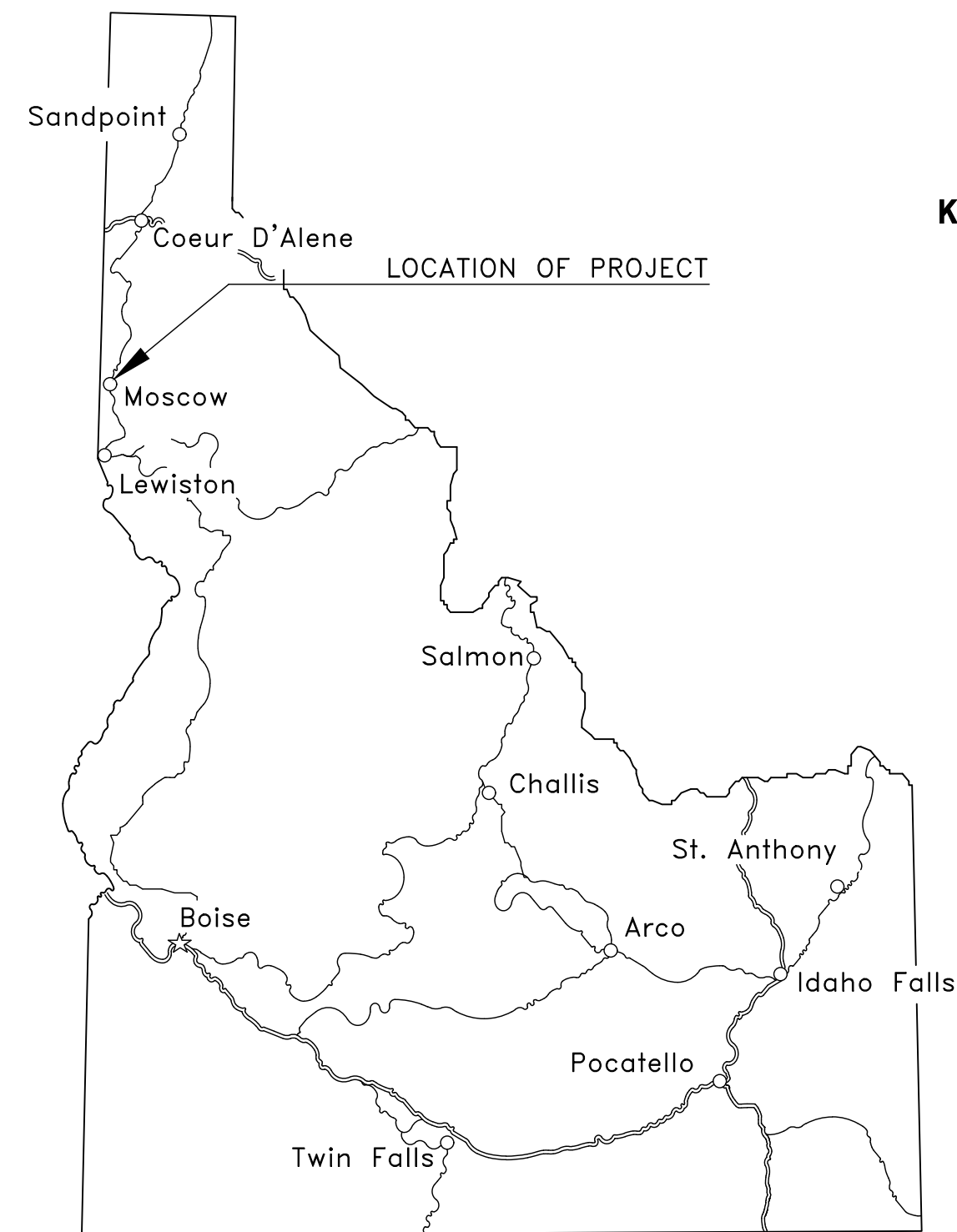
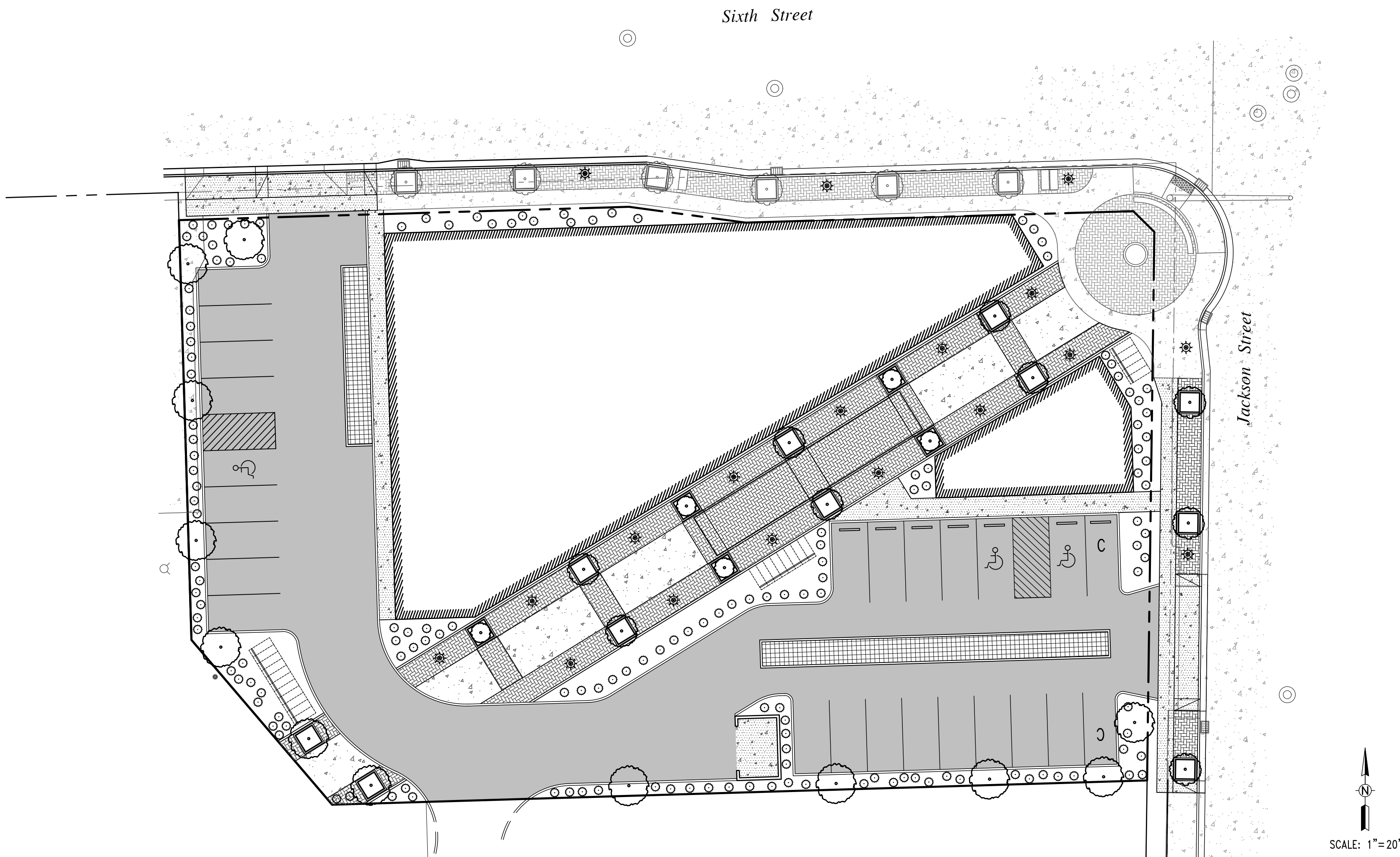
A-7

MOSCOW FLATIRON DEVELOPMENT

CITY OF MOSCOW LATAH COUNTY, IDAHO

BASIS OF ELEVATION
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THE BASE POSITION BEING CORRECTED BY NGS ONLINE POSITIONING USER SERVICE (OPUS).
NAVD88 IS 3.6± FEET HIGHER THAN NGVD29 (THE DATUM USED BY THE CITY OF MOSCOW).

DRAWING INDEX	
Sheet Number	Sheet Title
C1	COVER SHEET
C2	EROSION CONTROL AND GRADING PLAN
C3	UTILITY LAYOUT PLAN
C4	GENERAL NOTES AND DETAILS



Hedge & Associates, Inc.
Engineers • Planners • Landscape Architects

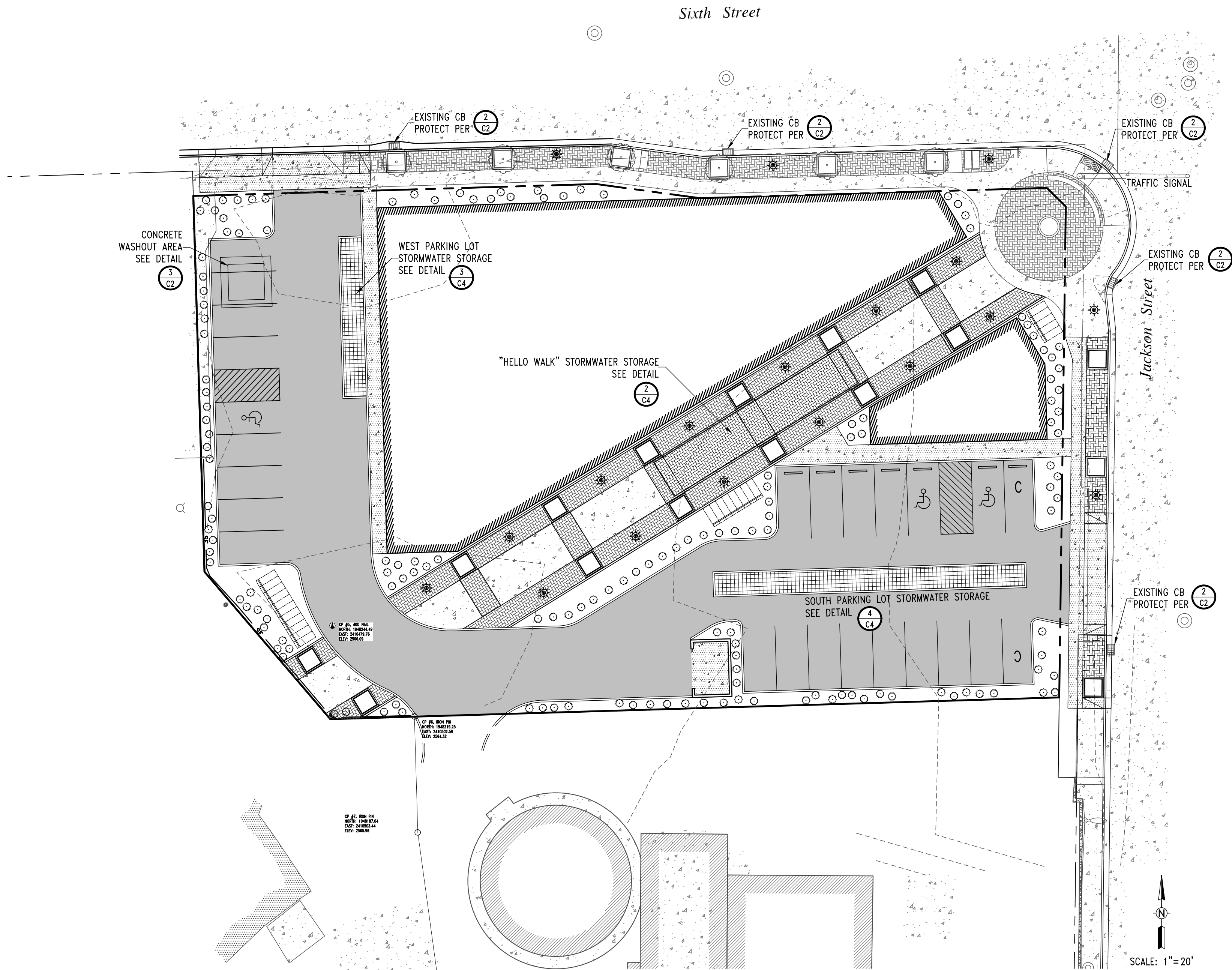
P.O. Box 8728
405 S. Washington Street
Moscow, Idaho 83843
(208) 882-5520

**PRELIMINARY
NOT FOR
CONSTRUCTION**

MOSCOW FLATIRON DEVELOPMENT
COVER SHEET
MOSCOW FLATIRON LLC / RUSTY OLPS
SIXTH STREET & JACKSON STREET
MOSCOW, IDAHO

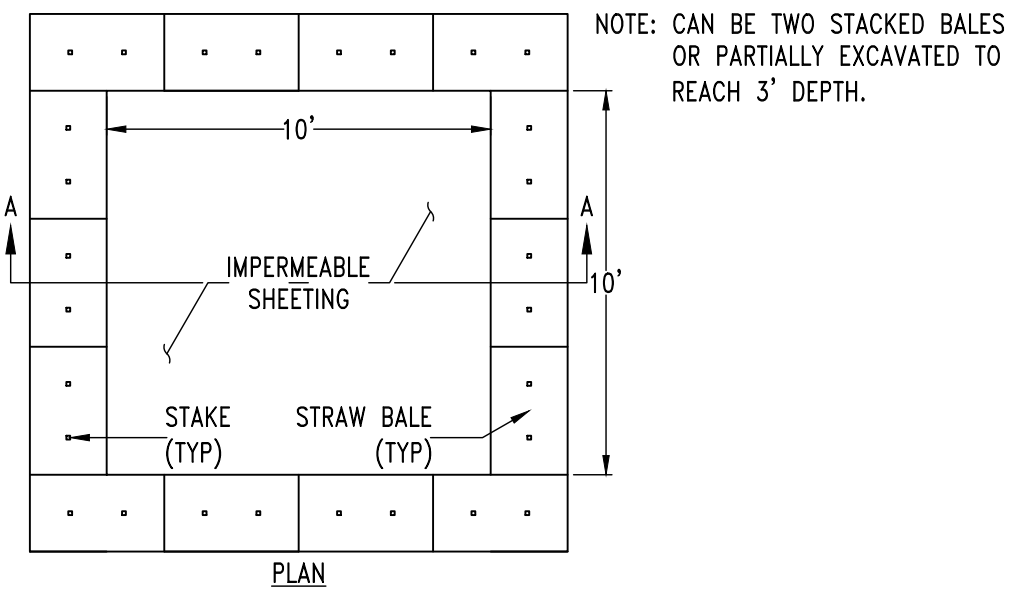
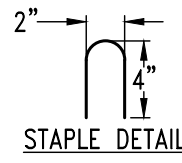
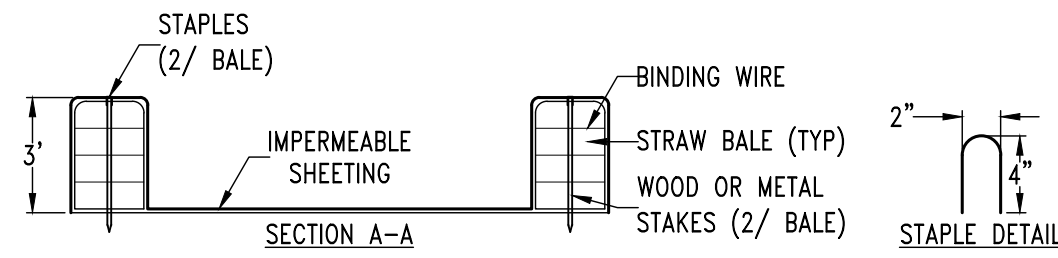
Designed by:	NN
Drafted by:	NN/SW
Checked by:	NN/SB
File Name:	4264 COVER.dwg
Tab:	COVER
Layer Style:	STANDARD
Plot Style:	OCE.CTB
Project:	4264-02-20
Date:	3/16/2020

C1



LEGEND

	40d NAIL CONTROL POINT (CP)
	5/8" IRON ROD
	SPOT ELEVATION - PROPOSED FINISHED GRADE
	SPOT ELEVATION - TOP BACK OF CURB/ASPHALT
	PROPERTY LINE
	RIGHT-OF-WAY
	EXISTING 1' CONTOUR INTERVAL
	PROPOSED 1' CONTOUR INTERVAL
	SILT FENCE - SEE DETAIL 1 C2
	CURB
	CONCRETE PAVING
	ASPHALT PAVING



CONSTRUCTION SPECIFICATIONS

1. LOCATE WASHOUT STRUCTURE A MINIMUM OF 50' AWAY FROM OPEN CHANNELS, STORM DRAIN INLETS, SENSITIVE AREAS, WETLANDS, BUFFERS AND WATER COURSES AND AWAY FROM CONSTRUCTION TRAFFIC.
2. SIZE WASHOUT STRUCTURE FOR VOLUME NECESSARY TO CONTAIN WASH WATER AND SOLIDS AND MAINTAIN AT LEAST 4" OF FREEBOARD. TYPICAL DIMENSIONS ARE 10'x10'x3' DEEP.
3. PREPARE SOIL BASE FREE OF ROCK OR OTHER DEBRIS THAT MAY CAUSE TEARS OR HOLES IN THE LINER. FOR LINER, USE 10 MIL OR THICKER UV RESISTANT, IMPERMEABLE SHEETING, FREE OF HOLES AND TEARS OR OTHER DEFECTS THAT COMPROMISE IMPERMEABILITY OF THE MATERIAL.
4. PROVIDE A SIGN FOR THE WASHOUT IN CLOSE PROXIMITY TO THE FACILITY.
5. KEEP CONCRETE WASHOUT STRUCTURE WATER TIGHT. REPLACE IMPERMEABLE LINER IF DAMAGED (E.G., RIPPED OR PUNCTURED). EMPTY OR REPLACE WASHOUT STRUCTURE THAT IS 75% FULL, AND DISPOSE OF ACCUMULATED MATERIAL PROPERLY. DO NOT REUSE PLASTIC LINER. WET-VACUUM STORED STORED LIQUIDS THAT HAVE NOT EVAPORATED AND DISPOSE OF IN AN APPROVED MANNER. PRIOR TO FORECASTED RAINSTORM, REMOVE LIQUIDS OR COVER STRUCTURE TO PREVENT OVERFLOWS. REMOVE HARDENED SOLIDS, WHOLE OR BROKEN UP, FOR DISPOSAL OR RECYCLING. MAINTAIN RUNOFF DIVERSION AROUND EXCAVATED WASHOUT STRUCTURE UNTIL STRUCTURE IS REMOVED.

ONSITE CONCRETE WASHOUT STRUCTURE

BASIS OF ELEVATION

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GRADING NOTES

ESTIMATED GRADING QUANTITIES

RAW CUT VOLUME = XXX C.Y.

RAW FILL VOLUME = XXX C.Y.

1. THIS PLAN IS ACCURATE FOR GRADING AND THE RELATED EROSION AND SEDIMENT CONTROL PURPOSES ONLY.
2. ALL AREAS TO RECEIVE EMBANKMENT (FILL) SHALL FIRST BE CLEARED AND GRUBBED.
3. ALL TOPSOIL SHALL BE EXCAVATED AND REMOVED FROM EMBANKMENT (FILL) AREAS. EXCAVATED TOPSOIL MATERIAL MAY BE STOCKPILED ON-SITE FOR USE AS LANDSCAPING MATERIAL - PROVIDE SILT FENCE AROUND STOCKPILES.
4. BORROW FOR EMBANKMENT (FILL) SHALL CONSIST OF NATIVE PALOUSE LOESS OR CLAY, OR ANY APPROVED GRAVELLY MATERIAL. ALL BORROW SHALL BE FREE OF ROOTS, BARK, TOPSOIL, ORGANIC OR OTHER DELETERIOUS MATERIALS.
5. COMPACT ALL EMBANKMENT (FILL) TO 95% MINIMUM OF THE PROCTOR VALUE DENSITY DETERMINED BY THE AASHTO T-99 METHOD. THE METHOD FOR CONSTRUCTING EMBANKMENT (FILL) SHALL BE SUCH THAT 95% MINIMUM DENSITIES ARE ACHIEVED THROUGHOUT.
6. WHERE EMBANKMENT (FILL) IS PLACED ON AN EXISTING SLOPE 4:1 OR STEEPER THE EXISTING SLOPE SHALL BE BENCHED HORIZONTAL PRIOR TO PLACING BORROW. THE WIDTH OF THE BENCHES SHALL NOT BE LESS THAN FIVE (5) FEET IN WIDTH.
7. COMPACTION TESTING SHALL BE PROVIDED BY THE OWNER. COMPACTION TEST LOCATIONS SHALL BE RECORDED ON A SITE PLAN SHOWING TEST NUMBER, DATE, AND ELEVATION OF EACH TEST. A REPORT OF THESE TESTS SHALL BE PROVIDED TO THE OWNERS, ENGINEER, CONTRACTOR, AND CITY. THERE SHALL BE A SUFFICIENT NUMBER OF TESTS TO ASSURE THAT 95% MINIMUM COMPACTION HAS BEEN ACHIEVED THROUGHOUT ALL EMBANKMENT (FILL) AREAS.
8. OWNER IS RESPONSIBLE TO SECURE ALL NECESSARY AUTHORIZATION FOR GRADING ACTIVITIES REQUIRED ON ADJACENT PROPERTIES.
9. A CITY OF MOSCOW GRADING PERMIT IS REQUIRED.
10. COMPLIANCE WITH NPDES PHASE II IS REQUIRED.

EROSION CONTROL NOTES

1. EROSION CONTROL MEASURES AS SET FORTH IN THIS PLAN SHALL APPLY TO ALL CONSTRUCTION ACTIVITY RELATIVE TO THIS SITE INCLUDING BUT NOT LIMITED TO, ALL EARTHWORK AND INSTALLATION OF PUBLIC IMPROVEMENTS.
2. SHOULD THE CITY ENGINEER DETERMINE THAT THE EROSION CONTROL MEASURES INSTALLED AS INDICATED ON THIS EROSION CONTROL PLAN ARE INADEQUATE, THE CITY ENGINEER SHALL DIRECT THE IMPLEMENTATION OF ADDITIONAL EROSION CONTROL MEASURES (AT THE EXPENSE OF THE DEVELOPER) TO ALLEVIATE INADEQUACIES.
3. THE OWNER AND/OR DEVELOPER OF THE PROPERTY IS ULTIMATELY RESPONSIBLE FOR THE FURNISHING, INSTALLATION AND MAINTENANCE OF ALL EROSION CONTROL MEASURES NECESSARY FOR FULL COMPLIANCE WITH MOSCOW CITY CODE.

IT IS NECESSARY FOR ANY PROJECT LARGER THAN ONE ACRE IN SIZE TO HAVE AN EPA NOTICE OF INTENT FOR STORMWATER DISCHARGES ASSOCIATED WITH CONSTRUCTION ACTIVITY UNDER AN NPDES GENERAL PERMIT (NOI PHASE II) FORM ON FILE WITH THE EPA STORM WATER NOTICE PROCESSING CENTER AT:

FOR REGULAR U.S. MAIL DELIVERY:
EPA STORMWATER NOTICE PROCESSING CENTER
MAIL CODE 4203M
U.S. EPA
1200 PENNSYLVANIA AVENUE, NW
WASHINGTON, DC 20460

FOR OVERNIGHT/EXPRESS MAIL DELIVERY:
EPA STORMWATER NOTICE PROCESSING CENTER
ROOM 7420
U.S. EPA
1201 CONSTITUTION AVENUE, NW
WASHINGTON, DC 20004

PRIOR TO CONSTRUCTION ACTIVITIES:

1. INFORM CITY, AND OTHER INSPECTORS, OF WORK SCHEDULED FOR WEEKENDS OR HOLIDAY.
2. CONSTRUCT GRAVEL PADS AT ENTRANCES TO SITE, AS INDICATED ON PLAN.
3. PLACE SILT FENCING AND CHECK DAMS AS INDICATED ON PLAN PRIOR TO ANY EXCAVATION OR EMBANKMENT WORK. IN GENERAL, CHECK DAMS SHALL BE PLACED AT MINIMUM 100 FOOT SPACING IN ALL SWALES AND DITCHES AND AT MINIMUM 50 FOOT SPACING ALONG SILT FENCE. ON SLOPES GREATER THAN 10% CHECK DAMS SHALL BE MADE OF ROCK, STRAW BALES, OR SILT FENCE, PER MOSCOW CITY STANDARDS.
4. GRADE ACCESS ROAD(S) TO ROADWAY CROSS-SLOPE TO PROVIDE DRAINAGE TO ROADSIDE DITCHES.
5. PLACE SILT FENCING AND STRAW BALES TO PROTECT EXISTING STORM WATER DRAINAGE FACILITIES THAT WILL BE IMPACTED BY THIS WORK. PROTECT EXISTING FACILITIES ON ADJACENT PROPERTY, AS NECESSARY. EXISTING CATCH BASINS SHALL BE PROTECTED PER DETAIL 2, THIS PAGE. MAINTAIN UNIMPEDED FLOW INTO CATCH BASINS THROUGHOUT CONSTRUCTION.
6. CONSTRUCT TEMPORARY SEDIMENT POND(S) IN THE LOCATION(S) AS SHOWN ON THE PLAN, INCLUDING DIRECTIVE DITCHES ALSO AS SHOWN THEREON AND/OR OTHERWISE REQUIRED.

DURING CONSTRUCTION ACTIVITIES AND WINTER SHUTDOWN, AS NECESSARY:

1. SUSPEND ALL EXCAVATION AND EMBANKMENT WORK DURING RAIN STORMS TO MINIMIZE EROSION AND SEDIMENT IMPACTS. MONITOR SITE CONDITIONS DURING AND FOLLOWING RAIN EVENTS FOR EROSION AND SEDIMENT CONTROL PROBLEMS - INSTALL CORRECTIVE MEASURES AND INITIATE CLEANUP MEASURES AS REQUIRED.
2. SPRAY WATER ON DISTURBED AREAS TO CONTROL DUST EROSION DURING DRY SEASON.
3. CONSTRUCT STORM WATER FACILITIES AS EARLY AS POSSIBLE DURING CONSTRUCTION. PROTECT NEW STORM WATER FACILITIES SIMILAR TO EXISTING (SEE NOTE 5 OF "PRIOR TO CONSTRUCTION ACTIVITIES").
4. SEED AND MULCH ALL DISTURBED AREAS.
5. ALL GRADED AREAS, FINISHED OR UNFINISHED, SHALL HAVE ACCEPTABLE EROSION CONTROL MEASURES APPLIED PRIOR TO OCTOBER 1 - NO EXCEPTIONS WITHOUT PRIOR WRITTEN CITY APPROVAL. ACCEPTABLE EROSION CONTROL MEASURES SHALL BE CONSIDERED ONE OF THE FOLLOWING:
 - a. HYDRAULICALLY APPLIED MIXTURE OF MULCH AND TACKIFIER - OF A TYPE AND APPLICATION RATE RECOMMENDED FOR MOSCOW AREA OPEN SOIL EROSION PROTECTION - SEED TO BE INCLUDED IF AREA IS FINISH GRADED.
 - b. AREAS SLOPING LESS THAN 4:1 TILLED WITH A FARM IMPLEMENT SUCH AS A DISC, PLOW OR SPRING TOOTH THAT RESULTS IN COARSE SURFACE TEXTURE.
 - c. STRAW SPREAD AND TRACKED INTO THE SURFACE. TRACK PERPENDICULAR TO THE DIRECTION OF FLOW.
 - d. EROSION CONTROL BLANKETS - INCLUDING AN APPROVED GRASS SEED/FERTILIZER MIX IF AREA IS FINISH GRADED.
 - e. OTHER METHODS AS SPECIFICALLY APPROVED BY THE CITY ENGINEER.

6. ALL SLOPES 4:1 OR STEEPER SHALL BE CAT-TRACKED PERPENDICULAR TO THE DIRECTION OF FLOW PRIOR TO OCTOBER 1 - IN ADDITION TO ONE OF THE ACCEPTABLE EROSION CONTROL MEASURES SET FORTH IN NO. 5 ABOVE.
7. SEED MIXTURES SHALL INCLUDE FERTILIZERS OF THE TYPE AND RATE OF APPLICATION AS RECOMMENDED BY GENERALLY ACCEPTED AREA PRACTICES.
8. PRIOR TO OCTOBER 1 CONTINUOUS EROSION CONTROL BLANKETS PLUS STRAW BALES AND/OR ROCK CHECK DAMS @ 100 FOOT INTERVALS SHALL BE INSTALLED IN ALL AREAS HAVING A POTENTIAL FOR CONCENTRATED WATER FLOW (I.E. SWALES AND DRAINAGE CHANNELS).
9. SEDIMENT PONDS INCLUDING CUTOFF / DIRECTIVE DITCHING, AS REQUIRED AND AS SHOWN ON THE PLANS SHALL BE IN PLACE AND OPERATIONAL PRIOR TO OCTOBER 1.

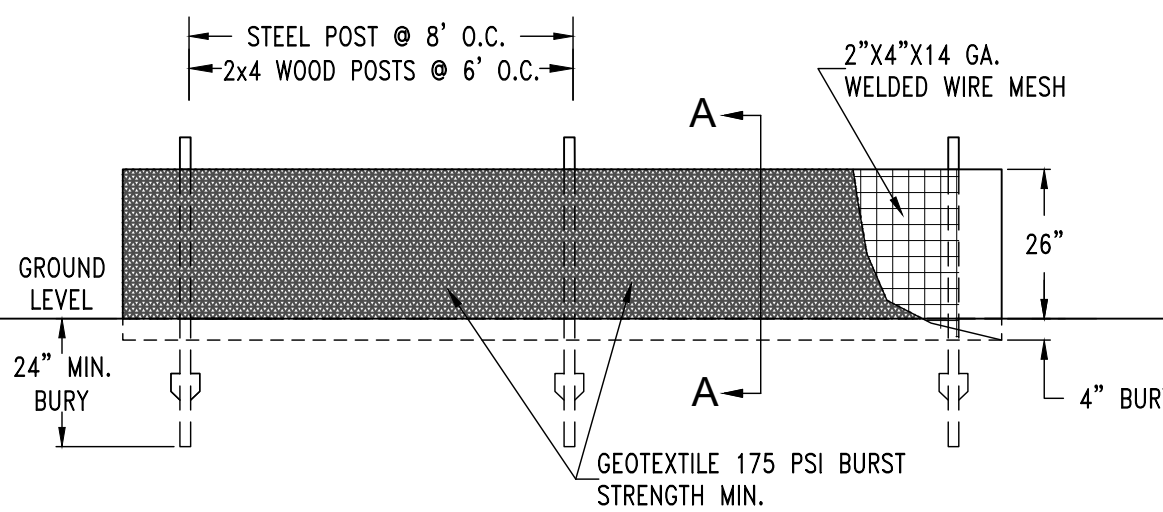
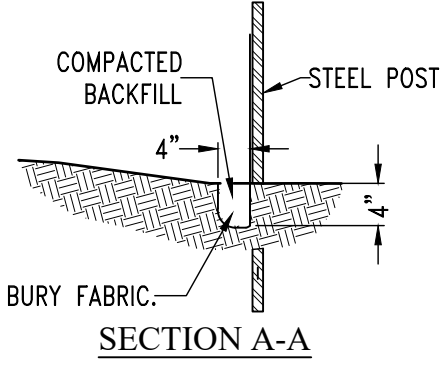
UPON COMPLETION OF CONSTRUCTION ACTIVITIES:

1. SILT FENCING SHALL REMAIN AFTER THE COMPLETION OF CONSTRUCTION UNTIL SUFFICIENT VEGETATION IS ESTABLISHED TO PREVENT EROSION AND MOBILIZATION OF SEDIMENT, AND UNTIL APPROVAL FOR REMOVAL IS OBTAINED FROM THE CITY ENGINEER.
2. PERIODIC MAINTENANCE OF EROSION CONTROL MEASURES IS REQUIRED UNTIL PERMANENT DRAINAGE FACILITIES ARE OPERATIONAL AND SUFFICIENT VEGETATION IS ESTABLISHED. SUCH MAINTENANCE INCLUDES, BUT IS NOT LIMITED TO: CLEANING OF CATCH BASINS AND STORM SEWER MANHOLES; MAINTAINING SILT FENCING AND STRAW BALE STRUCTURE; REMOVAL OF ACCUMULATED SOIL; AND STREET CLEANING. A TYPICAL SCHEDULE FOR SUCH MAINTENANCE WOULD BE AT LEAST ONCE PER MONTH AND AFTER ALL SIGNIFICANT STORM EVENTS.

UPON ESTABLISHMENT OF SUFFICIENT VEGETATION AND PERMANENT DRAINAGE FACILITIES OPERATION:

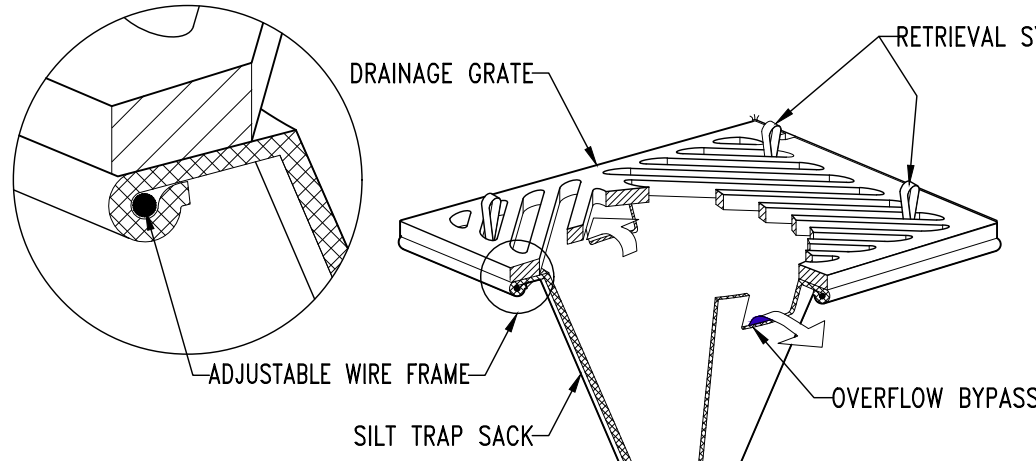
1. REMOVE ALL EROSION CONTROL MEASURES UPON RECEIPT OF APPROVAL FOR SUCH REMOVAL FROM CITY ENGINEER. CLEAR STREETS AND SIDEWALKS OF ACCUMULATED SILT, MUD, AND DEBRIS.
2. CONTINUED EROSION CONTROL IS THE PROPERTY OWNER'S RESPONSIBILITY AFTER EROSION CONTROL MEASURES ARE REMOVED, AND RECEIPT OF APPROVAL FROM THE CITY ENGINEER.

FENCE WITH 2X2 PRE-INSTALLED POSTS IS NOT ALLOWED

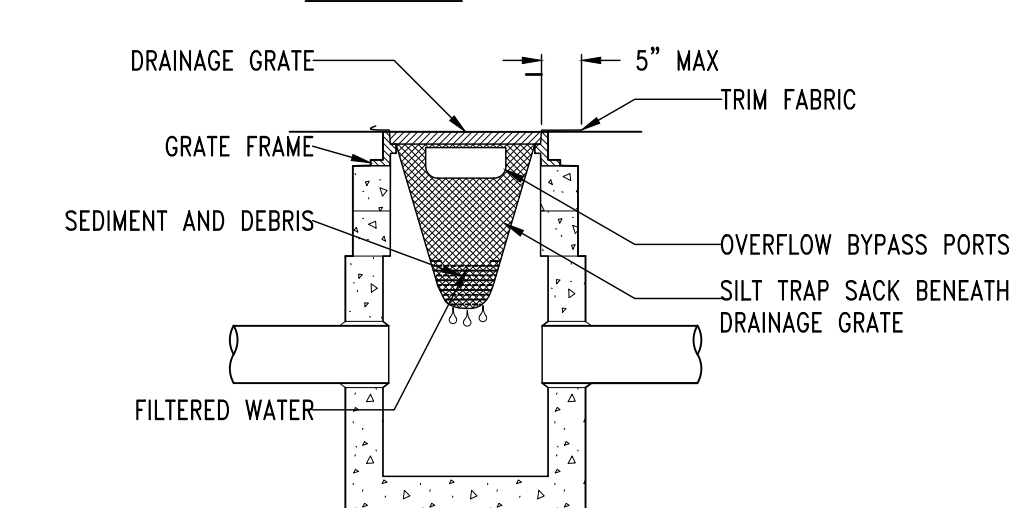


- SILT FENCE NOTES:**
1. ATTACH WIRE MESH TO POSTS WITH APPROVED METAL DEVICES.
 2. ATTACH GEOTEXTILE FABRIC TO THE UP-SLOPE SIDE OF THE POSTS AND WIRE MESH TOP AND BOTTOM WITH APPROVED METAL FASTENERS @ 24" O.C. MAXIMUM SPACING.
 3. GEOTEXTILE FABRIC TOP EDGE SHALL BE 26" MINIMUM ABOVE GROUND.
 4. OVERLAP VERTICAL SILT FENCE SPLICES 8" MINIMUM (POST TO POST). SEE ELEVATION, ABOVE. AVOID SUMPS AND OTHER LOW POINTS.

SILT FENCING DETAIL



DETAILS



SECTION

NOT TO SCALE

NOTES:

1. SIZE AND SHAPE OF THE SILT TRAP SACK TO FIT THE STORM STRUCTURE IT WILL SERVICE. (RECTANGULAR OR ROUND).
2. THE SILT TRAP SACK SHALL HAVE A BUILT-IN HIGH-FLOW RELIEF SYSTEM (OVERFLOW BYPASS).
3. THE SILT TRAP SACK ASSEMBLY MUST ALLOW REMOVAL WITHOUT SPILLING THE COLLECTED MATERIAL.
4. EMPTY SILT TRAP SACK AND DISPOSE OF SEDIMENT AND DEBRIS BEFORE THE SACK IS HALF-FULL.
5. ENSURE THE SILT TRAP SACK ASSEMBLY DOES NOT SPILL OR FALL INTO THE STORM STRUCTURE. IF SEDIMENT IS SPILLED INTO THE STORM STRUCTURE, REMOVE THE SPILLED MATERIAL BY SUCTION HOSE OR OTHER APPROVED METHOD.

CATCH BASIN PROTECTION



Know what's below.
Call before you dig.

PRELIMINARY
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CONSTRUCTION

MOSCOW FLATIRON DEVELOPMENT
EROSION CONTROL AND GRADING PLAN
MOSCOW FLATIRON LLC / RUSTY OLPS
SIXTH STREET & JACKSON STREET
MOSCOW, IDAHO

Designed By:	NN
Drafted by:	NN/SW
Checked by:	NN/SB
File Name:	4264 GRADING.dwg
Tab:	GRADING
Layer Style:	STANDARD
Plot Style	OCE.CTB
Project:	4264-02-20
Date:	3/16/2020

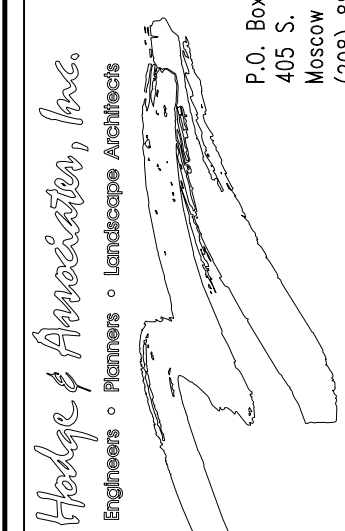
C2

NOTES:

1. PLACEMENT OF LANDSCAPING IN OFF-STREET PARKING LOTS:
REQUIRED EXTERIOR LANDSCAPING: LANDSCAPING SHALL BE PROVIDED BETWEEN OFF-STREET PARKING LOTS
AND ABUTTING PUBLIC RIGHT-OF-WAY AND ADJACENT PROPERTIES. THE BUFFER STRIP IN WHICH THE LANDSCAPING
IS PROVIDED SHALL BE A MINIMUM OF THREE (3') IN WIDTH AND SHALL CONTAIN:
1. ONE (1) TREE OF ONE AND ONE-HALF INCH (1.5") CALIPER EVERY FORTY (40) LINEAL FEET
2. ONE (1) SHRUB OF AT LEAST ONE (1) GALLON IN SIZE FOR PERENNIAL AND AT LEAST TWO (2) GALLONS IN SIZE
FOR DECIDUOUS OR EVERGREEN, EVERY SIX FEET (6'). SUCH SHRUBS, UPON MATURITY, SHALL PROVIDE VISUAL
RELIEF OF A MINIMUM OF THREE FEET (3') IN HEIGHT AND FIFTY PERCENT (50%) SHALL BE EVERGREEN.
3. CONSIDERATION SHALL BE GIVEN TO THE SELECTION OF TREES AND SHRUBS THAT AT MATURITY DO NOT PRESENT
VEHICULAR SIGHT OBSTRUCTIONS AT DRIVEWAY / STREET INTERSECTIONS.



Know what's below.
Call before you dig.



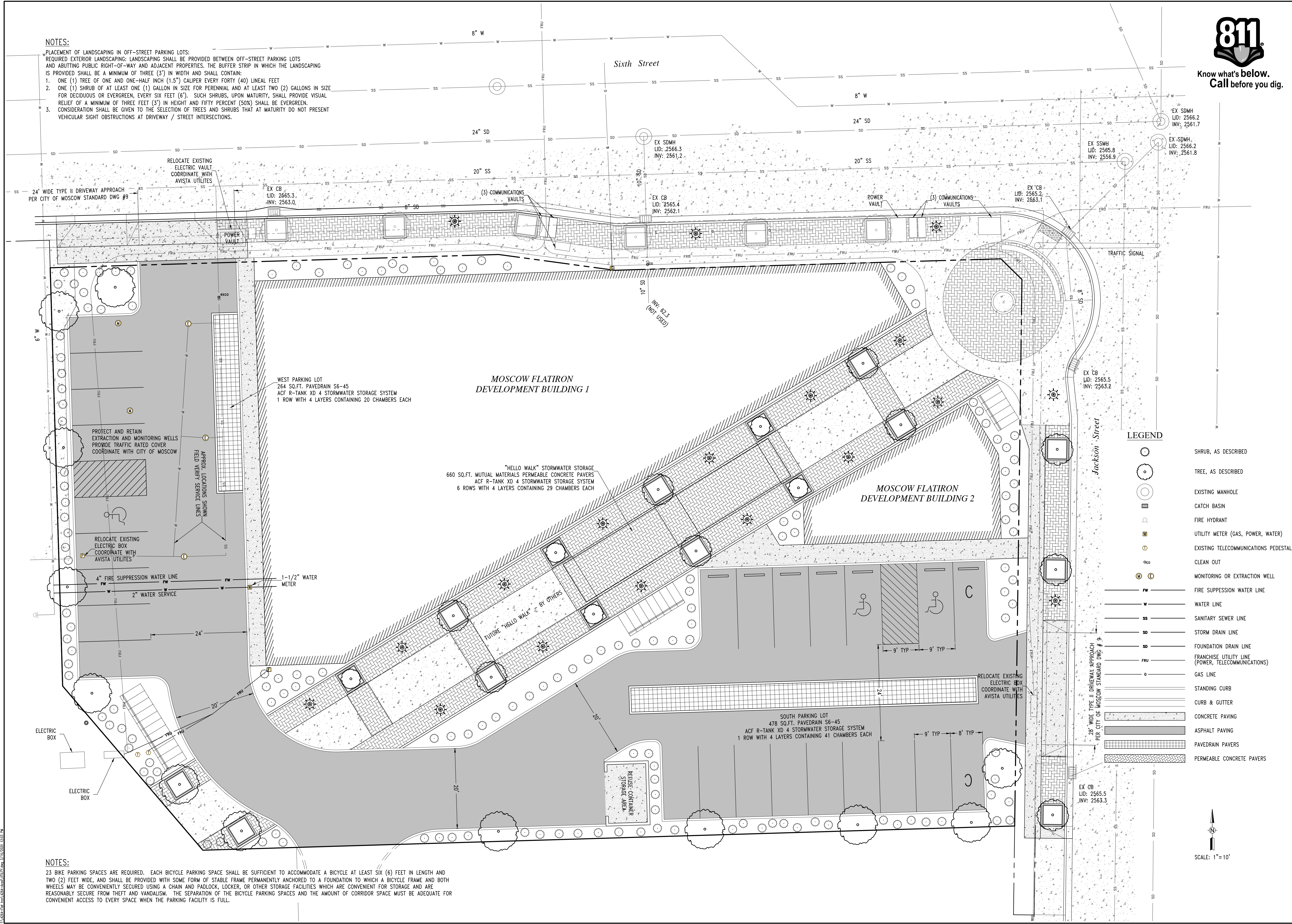
P.O. Box 8728
405 S. Washington Street
Moscow Idaho 83843
(208) 882-5520

PRELIMINARY
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CONSTRUCTION

MOSCOW FLATIRON DEVELOPMENT
UTILITY LAYOUT PLAN
MOSCOW FLATIRON LLC / RUSTY OLPS
SIXTH STREET & JACKSON STREET
MOSCOW, IDAHO

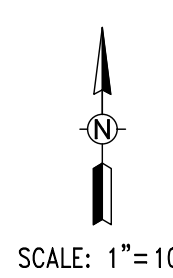
Designed By:	NN
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Checked by:	NN/SB
File Name:	4264 UTILITY.dwg
Tab:	UTILITY
Layer Style:	STANDARD
Plot Style:	OCE.CTB
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Date:	3/16/2020

C3



LEGEND

- SHRUB, AS DESCRIBED
- TREE, AS DESCRIBED
- EXISTING MANHOLE
- CATCH BASIN
- FIRE HYDRANT
- UTILITY METER (GAS, POWER, WATER)
- EXISTING TELECOMMUNICATIONS PEDESTAL
- CLEAN OUT
- MONITORING OR EXTRACTION WELL
- FIRE SUPPRESSION WATER LINE
- WATER LINE
- SANITARY SEWER LINE
- STORM DRAIN LINE
- FOUNDATION DRAIN LINE
- FRANCHISE UTILITY LINE (POWER, TELECOMMUNICATIONS)
- GAS LINE
- STANDING CURB
- CURB & GUTTER
- CONCRETE PAVING
- ASPHALT PAVING
- PAVEDRAIN PAVERS
- PERMEABLE CONCRETE PAVERS

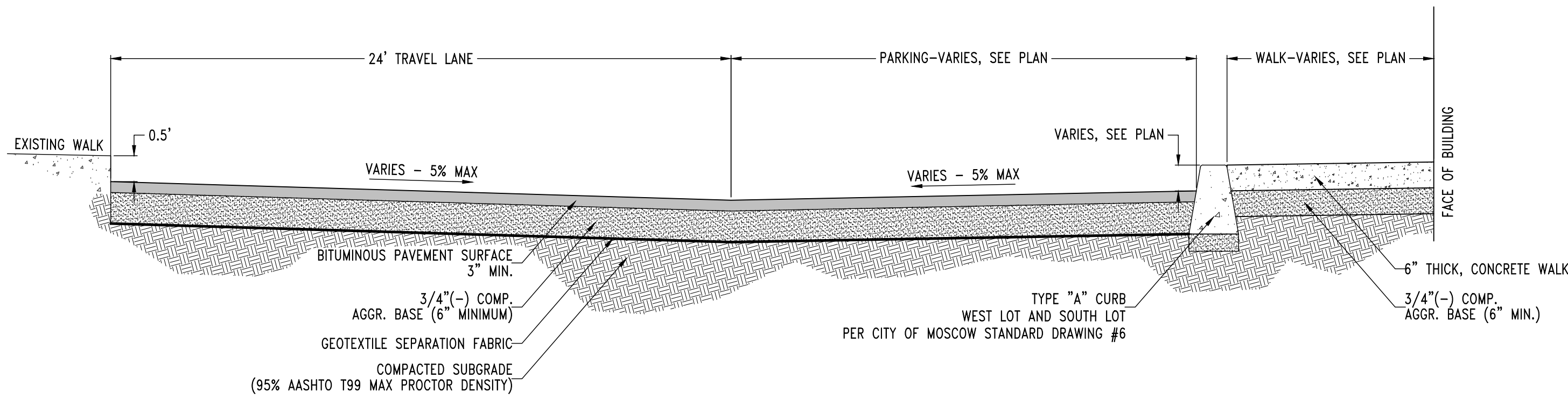


SCALE: 1"=10'

NOTES:

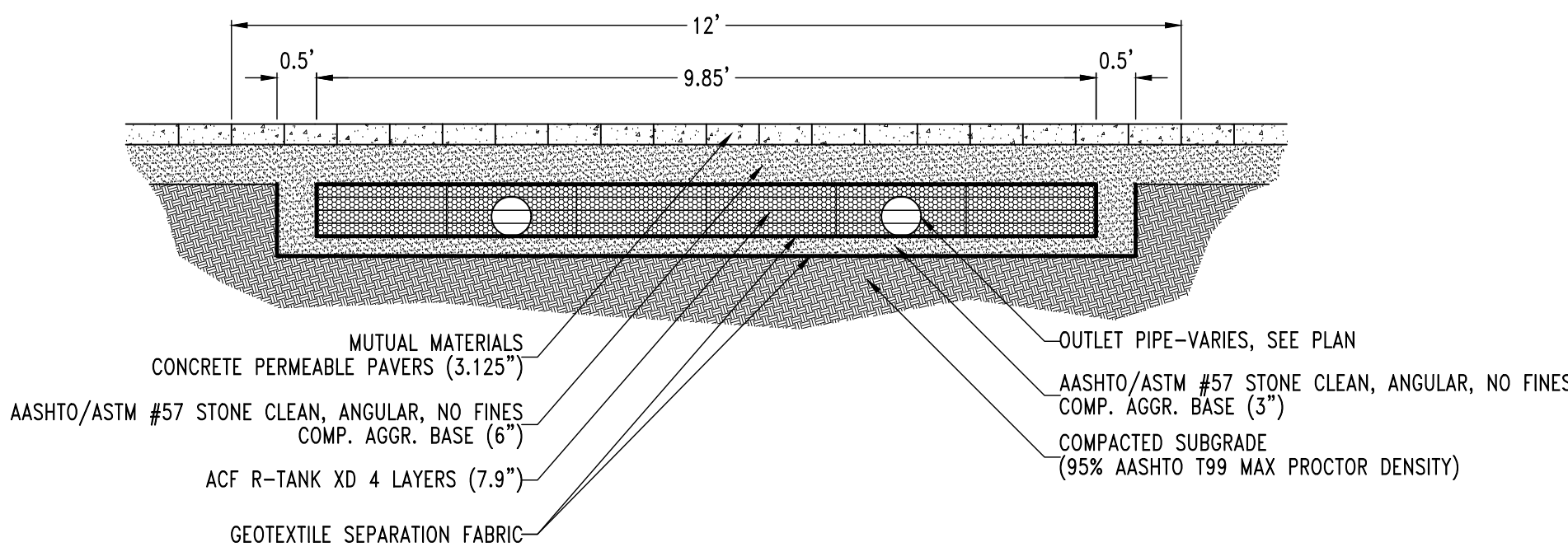
23 BIKE PARKING SPACES ARE REQUIRED. EACH BIKE PARKING SPACE SHALL BE SUFFICIENT TO ACCOMMODATE A BICYCLE AT LEAST SIX (6) FEET IN LENGTH AND TWO (2) FEET WIDE, AND SHALL BE PROVIDED WITH SOME FORM OF STABLE FRAME PERMANENTLY ANCHORED TO A FOUNDATION TO WHICH A BICYCLE FRAME AND BOTH WHEELS MAY BE CONVENIENTLY SECURED USING A CHAIN AND PADLOCK, LOCKER, OR OTHER STORAGE FACILITIES WHICH ARE CONVENIENT FOR STORAGE AND ARE REASONABLY SECURE FROM THEFT AND VANDALISM. THE SEPARATION OF THE BICYCLE PARKING SPACES AND THE AMOUNT OF CORRIDOR SPACE MUST BE ADEQUATE FOR CONVENIENT ACCESS TO EVERY SPACE WHEN THE PARKING FACILITY IS FULL.

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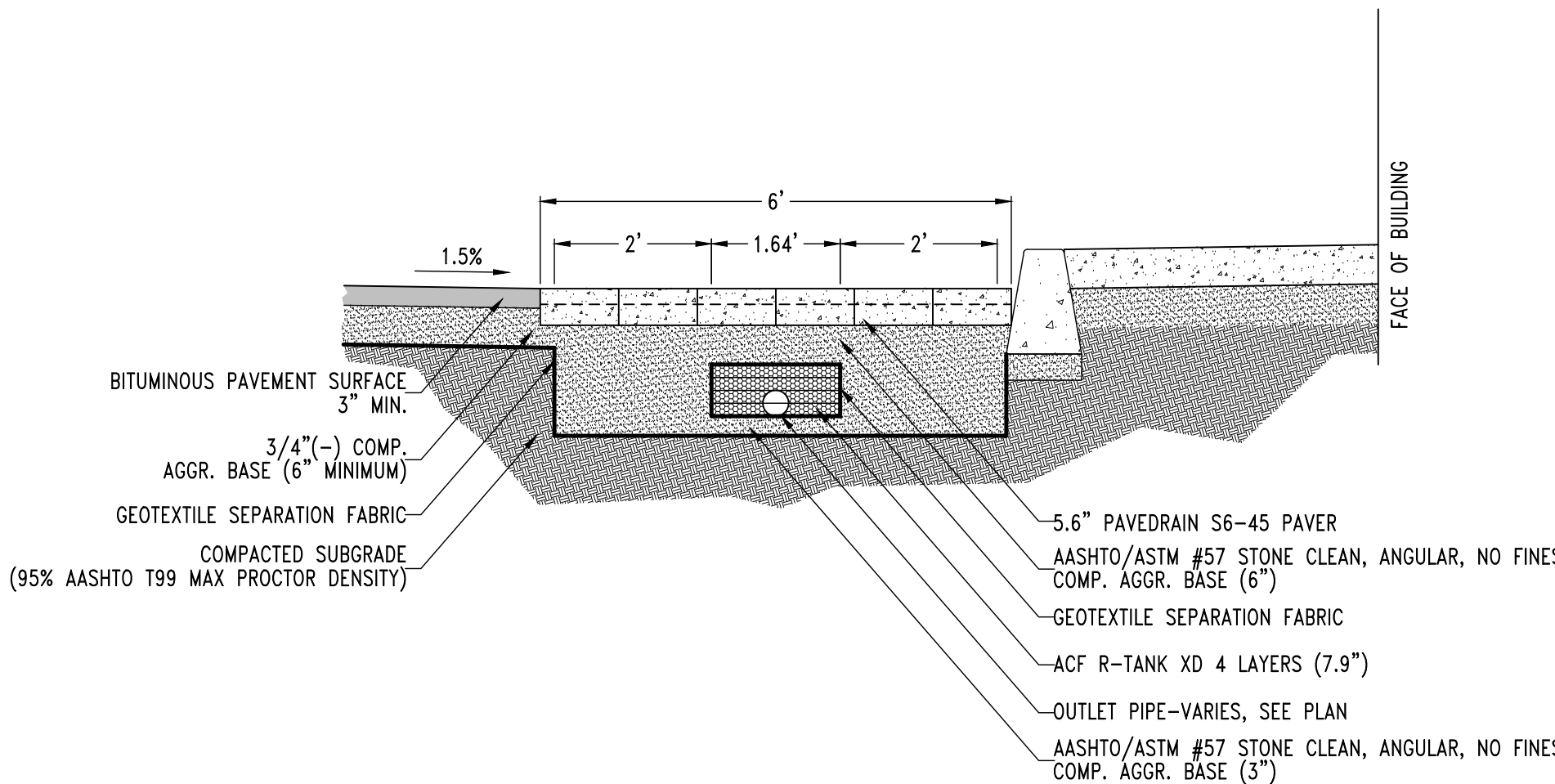


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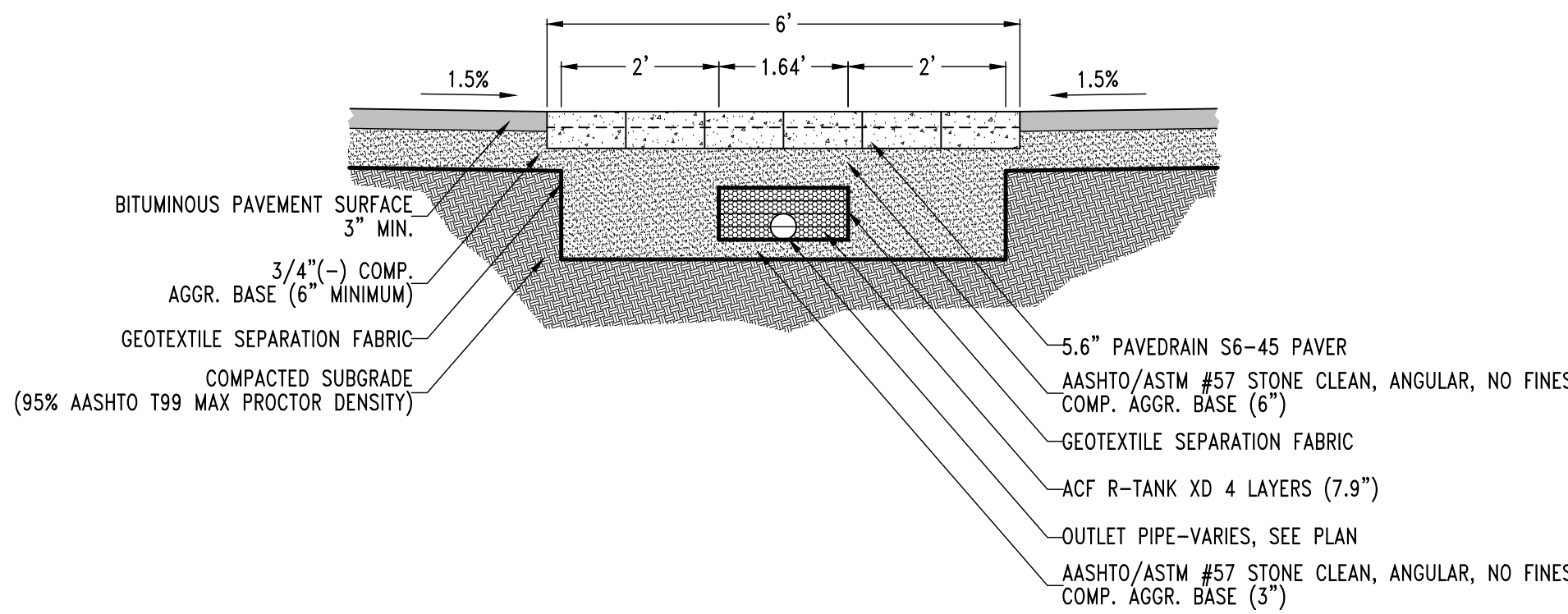
PARKING LOT
TYPICAL SECTION
NTS
1
C4



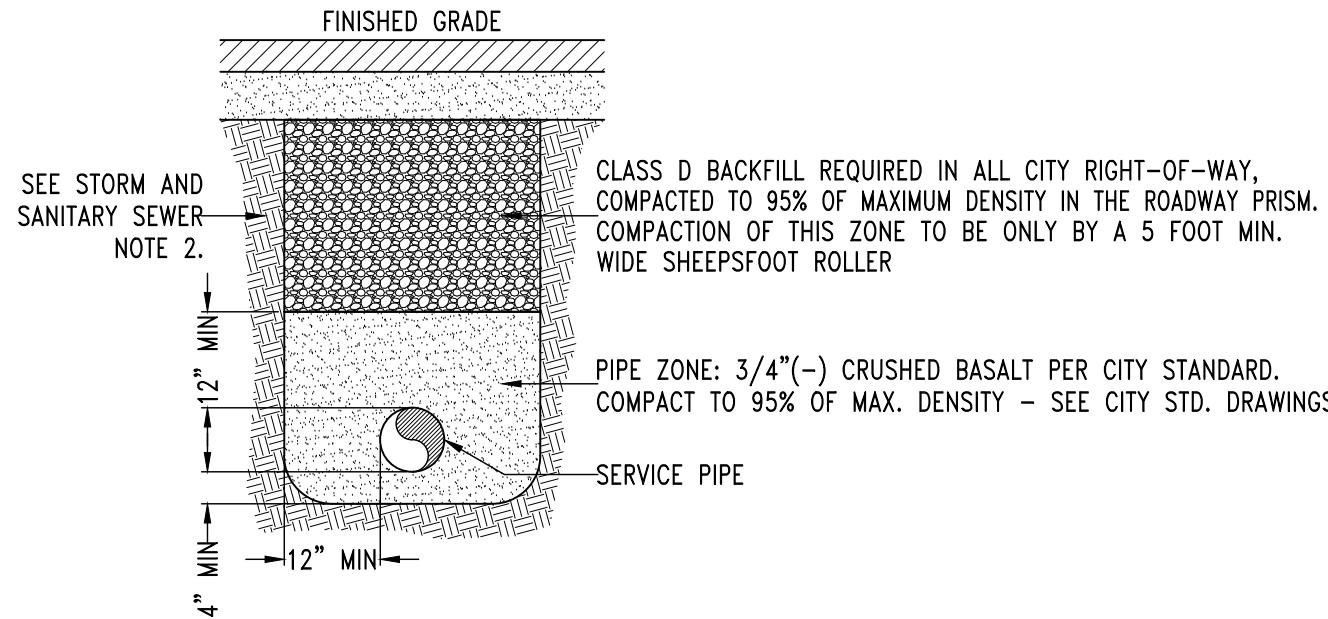
HELLO WALK STORMWATER STORAGE
TYPICAL SECTION
NTS
2
C4



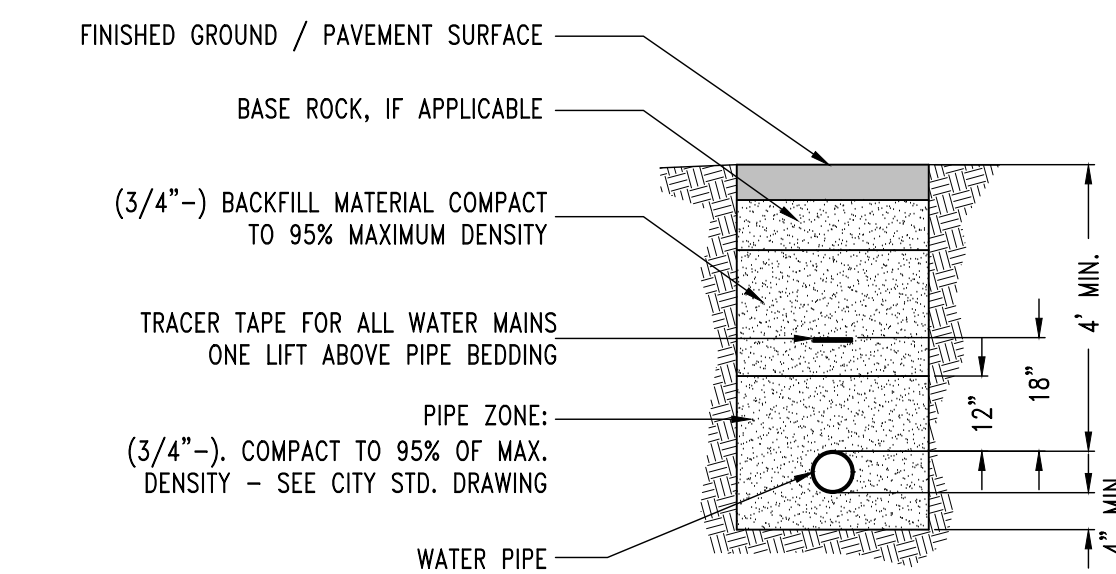
WEST PARKING LOT STORMWATER STORAGE
TYPICAL SECTION
NTS
3
C4



SOUTH PARKING LOT STORMWATER STORAGE
TYPICAL SECTION
NTS
4
C4



STORM OR SANITARY SEWER TRENCH
TYPICAL SECTION
NTS
5
C4



NOTE: WATER TRENCH TO COMPLY WITH CITY OF MOSCOW STANDARD CLASS 'D' BACKFILL

WATER LINE TRENCH
TYPICAL SECTION
NTS
6
C4

GENERAL NOTES

- ALL WORK TO BE IN ACCORDANCE WITH CITY OF MOSCOW STANDARDS.
- UNDERGROUND UTILITIES SHOWN ON THESE DRAWINGS ARE NOT GUARANTEED ACCURATE OR COMPLETE. CALL 811 FOR UTILITY LOCATES BEFORE BEGINNING CONSTRUCTION.
- ALL CONSTRUCTION MUST COMPLY WITH STATE OF IDAHO STANDARDS FOR DRINKING WATER SYSTEMS AND WITH THE STATE OF IDAHO DEPARTMENT OF ENVIRONMENTAL QUALITY CRITERIA FOR SEWAGE WORKS DESIGN.
- THE OWNER IS RESPONSIBLE FOR EROSION AND SEDIMENT CONTROL IN ACCORDANCE WITH ALL LOCAL, STATE, AND FEDERAL ORDINANCES, POLICIES AND LAWS. AN APPROVED EROSION CONTROL PLAN IS PROVIDED AS PART OF THESE CONSTRUCTION DRAWINGS.
- MAXIMUM DENSITY AND OPTIMUM MOISTURE CONTENT WILL BE DETERMINED USING AASHTO T99 EXCEPT AS NOTED. IN PLACE DENSITY AND MOISTURE CONTENT SHALL BE DETERMINED BY ASTM D 2922 (NUCLEAR DENSOMETER).
- THE DEVELOPER'S ENGINEER TO FURNISH DESCRIPTIONS FOR ALL PUBLIC UTILITY EASEMENTS OUTSIDE THE BOUNDARY OF THE PLAT. THE CITY WILL PREPARE THE EASEMENT DOCUMENTS.

ROADWAY

- ALL BACKFILL AND SUBGRADE IN PUBLIC STREET SECTION SHALL BE COMPACTED TO 95% OF MAXIMUM DENSITY.
- SEE TYPICAL SECTION 1, 2, & 3 THIS SHEET. PLACE COMBINATION CURB AND GUTTER, (SEE DWG #6 CITY OF MOSCOW STANDARD DRAWINGS) ON ALL PUBLIC STREETS.
- ADJUST ALL MANHOLES AND WATER VALVE BOXES TO FINISHED GRADE PRIOR TO PAVING.
- ALL OFFSET DIMENSIONS ARE FROM INDICATED STATION LINES UNLESS OTHERWISE NOTED.
- STREET MONUMENTS SHALL BE SET BY ENGINEER IN CASINGS PROVIDED AND SET BY CONTRACTOR. ENGINEER SHALL MARK LOCATION OF CASINGS FOR CONTRACTOR.
- ALL CUT OR FILL SLOPES ON STREET RIGHTS-OF-WAY SHALL BE MULCHED TO PREVENT EROSION. SEE EROSION CONTROL PLAN, SHEET C2.
- ALL CURB RADI AT INTERSECTIONS TO BE 25 FEET, PER CITY STANDARD, UNLESS SPECIFICALLY NOTED OTHERWISE.

STORM & SANITARY SEWER

- THE CITY OF MOSCOW IS OPERATOR OF WATER AND SEWER FACILITIES SERVING THIS DEVELOPMENT.
- ALL TRENCH AND FOUNDATION EXCAVATIONS GREATER THAN FOUR FEET DEEP SHALL HAVE SLOPES OF 1.5:1 (HORIZONTAL TO VERTICAL) OR, BRACING OR SHORING SHALL BE PROVIDED IN ACCORDANCE WITH OSHA AND OTHER APPROPRIATE LOCAL CODES.
- ALL SATURATED, LOOSE, OR DISTURBED SOIL SHALL BE REMOVED FROM THE BOTTOM OF UTILITY TRENCHES PRIOR TO PLACEMENT OF PIPE BEDDING. PIPE BEDDING AND TRENCH BACKFILL SHALL BE PLACED IN ACCORDANCE WITH CITY OF MOSCOW STANDARDS.
- SEWER STUBS SHALL BE INSTALLED AT A MINIMUM SLOPE OF 1/4' PER FOOT, AND CONNECTED TO MAINS USING MANUFACTURED WYES.
- SANITARY SEWER STUBS SHALL BE TERMINATED BY PROVIDING AN ELBOW AND CAPPED RISER TO 12" MINIMUM ABOVE FINISHED GRADE. TERMINATE AT PROPERTY LINE. MATERIAL SHALL BE GREEN (SDR 35) PVC. WRAP 'SEWER' LOCATE TAPE ON ENDS; TAPE FURNISHED BY CONTRACTOR.
- FOUNDATION DRAIN STUBS SHALL BE TERMINATED BY PROVIDING AN ELBOW & CAPPED RISER TO 12" MINIMUM ABOVE GRADE. TERMINATE AT PROPERTY LINE. MATERIAL SHALL BE WHITE PVC. WRAP 'STORM SEWER' LOCATE TAPE ON ENDS; TAPE FURNISHED BY CONTRACTOR.
- ALL STORM AND SANITARY SEWER LINES SHALL HAVE 2.0' MINIMUM COVER.
- USE ECCENTRIC CONES EXCEPT WHEN SPECIFICALLY APPROVED BY THE ENGINEER. PLACE THE CONE ENTRY HOLE ABOVE THE OUTLET PIPE. FLAT TOPS ARE NOT ALLOWED WITHOUT APPROVAL OF THE ENGINEER.
- SANITARY SEWER PIPE (SS): ALL SS PIPE TO BE SDR 35, PVC (GREEN), UNLESS OTHERWISE NOTED.
- STORM SEWER PIPE (SD): ALL SD PIPE TO BE SDR 35, PVC (WHITE), UNLESS OTHERWISE NOTED.

WATER

- WATER SERVICES SHALL BE DUG AND INSTALLED BY CONTRACTOR AT OWNER'S EXPENSE. CITY CREW WILL MAKE TAP TO MAIN. ALL WATER SERVICE MATERIALS FURNISHED BY CITY.
- ALL WATER SERVICES SHALL BE INSPECTED BY THE CITY PRIOR TO BACKFILLING TRENCHES.
- INSTALL "LOCATE" TAPE 18" ABOVE PIPE AND #10 LOCATE WIRE DIRECTLY ABOVE PIPE IN PVC WATER LINE TRENCH. REFER TO DETAIL, THIS SHEET.
- HYDROSTATIC TESTING AND DISINFECTION OF WATER MAINS SHALL BE DONE IN ACCORDANCE WITH CITY OF MOSCOW STANDARDS. THE CONTRACTOR SHALL REQUEST PIPE TESTING AT LEAST 48 HOURS IN ADVANCE OF THE TIME ACTUAL TESTS SHALL BE PERFORMED.
- BACK FLOW DEVICES SHALL BE PROVIDED PER CITY CODES AND POLICIES.
- FIRE HYDRANTS SHALL BE PLACED WITH THE STEAMER PORT FACING THE STREET OR DRIVEWAY AS APPROPRIATE AND SHALL BE MAINTAINED CLEAR AND ACCESSIBLE FOR FIRE PROTECTION DURING CONSTRUCTION. NO OBSTRUCTIONS SHALL BE PLACED WITHIN THREE FEET OF THE FIRE HYDRANT.
- NORMAL WATER PRESSURE AT LOWEST POINT IS 92 PSI; NORMAL WATER PRESSURE AT HIGHEST POINT IS 92 PSI.
- WATER PIPE: ALL WATER MAINS TO BE C 900 CLASS 150 PVC, UNLESS OTHERWISE NOTED.

SEPARATION OF WATER AND SEWER

- WATER AND SEWER (SANITARY OR STORM) MAINS SHALL BE SEPARATED BY A HORIZONTAL DISTANCE NO LESS THAN TEN (10) FEET. IN ANY INSTANCE WHERE SUCH SEPARATION IS NOT ACHIEVABLE, THE FOLLOWING STANDARDS SHALL BE MET:
 - THE WATER AND SEWER MAINS SHALL BE SEPARATED BY AT LEAST SIX (6) HORIZONTAL FEET MEASURED BETWEEN THE OUTSIDE WALLS OF THE PIPES, AND THE SEWER MAIN SHALL BE CONSTRUCTED TO WATER MAIN STANDARDS AT ALL SUCH POINTS; AND
 - THE WATER MAIN SHALL BE A MINIMUM OF EIGHTEEN (18) INCHES ABOVE THE SEWER MAIN.
- THE REQUIREMENTS FOR THE VERTICAL SEPARATION OF WATER AND SEWER MAINS ARE AS FOLLOWS:
 - AT ANY POINT WHERE THE SEWER AND WATER MAINS CROSS, THEY SHALL BE SEPARATED BY A VERTICAL DISTANCE OF NO LESS THAN EIGHTEEN (18) INCHES.
 - AT ANY POINT WHERE THE SEWER MAIN CROSSES ABOVE THE WATER MAIN THE SEWER MAIN SHALL BE SUPPORTED TO MAINTAIN LINE & GRADE.
 - AT ANY POINT WHERE THE SEWER AND WATER MAIN CROSS, THE CROSSING SHALL BE ARRANGED SO THAT THE WATER MAIN JOINTS WILL BE EQUAL DISTANCE AND AS FAR AS POSSIBLE FROM THE SEWER MAIN JOINTS.
 - IF THE WATER MAIN IS BELOW THE SEWER MAIN, THE SEWER MAIN SHALL BE CONSTRUCTED OF MATERIALS CONFORMING TO WATER MAIN STANDARDS AND PRESSURE TESTED TO 150 PSI TO ASSURE WATER TIGHTNESS PRIOR TO BACKFILLING IF THE EIGHTEEN (18) INCH VERTICAL SEPARATION CANNOT BE MAINTAINED.
 - IN LIEU OF CONSTRUCTING (OR RECONSTRUCTING) A NON-POTABLE WATER MAIN DUE TO LESS THAN EIGHTEEN (18) INCHES OF VERTICAL SEPARATION FROM A POTABLE WATER MAIN, EITHER THE NON-POTABLE MAIN OR THE POTABLE WATER MAIN MAY BE ENCASED WITH A SLEEVING MATERIAL ACCEPTABLE TO DEQ FOR A DISTANCE OF TEN (10) HORIZONTAL FEET ON BOTH SIDES OF THE CROSSING.

PRELIMINARY
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MOSCOW FLATIRON DEVELOPMENT
GENERAL NOTES AND DETAILS
MOSCOW FLATIRON LLC / RUSTY OLPs
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C4